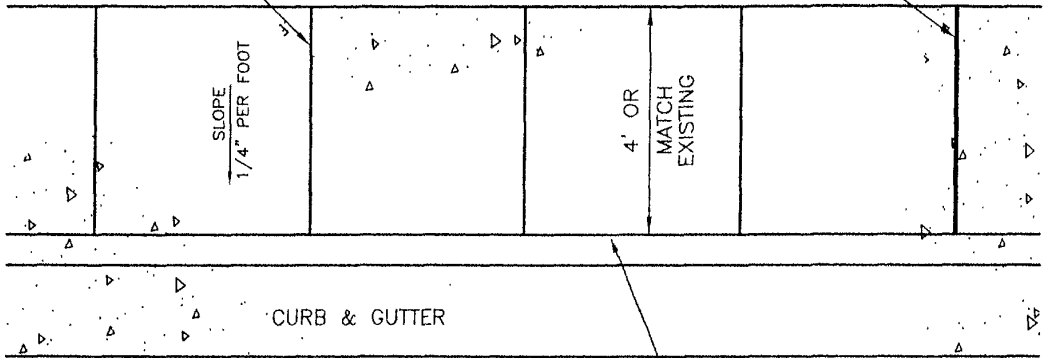


TOOLED JOINT SPACED TO MATCH SIDEWALK WIDTH ± MATCH CURB & GUTTER JOINT WHERE POSSIBLE

EXPANSION JOINT SPACED 100' MAX.

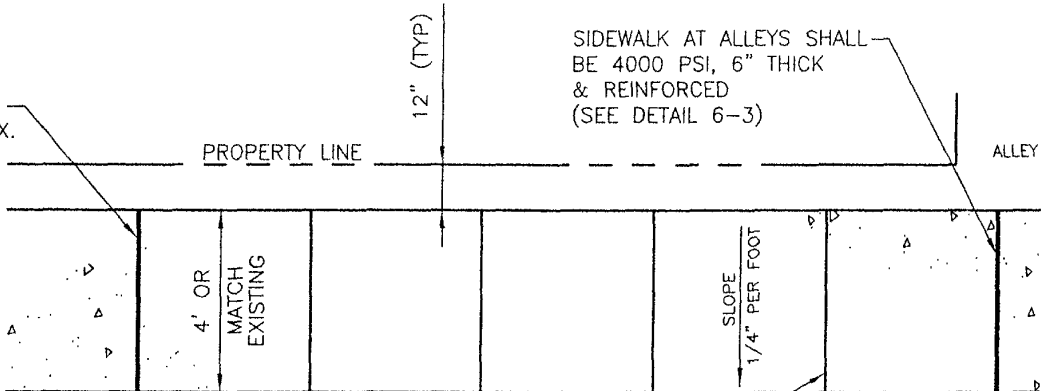


CURBSIDE SIDEWALK

TOOLED JOINT W/HOT-POURED ELASTIC SEALER OR APPROVED EQUAL

ALL SIDEWALKS SHALL BE A MINIMUM OF 4" THICK 4000 PSI CONCRETE WITH A MINIMUM OF 2" OF COMPACTED CUSHION MATERIAL PLACED UNDER THE SIDEWALK

EXPANSION JOINT SPACED 100' MAX.

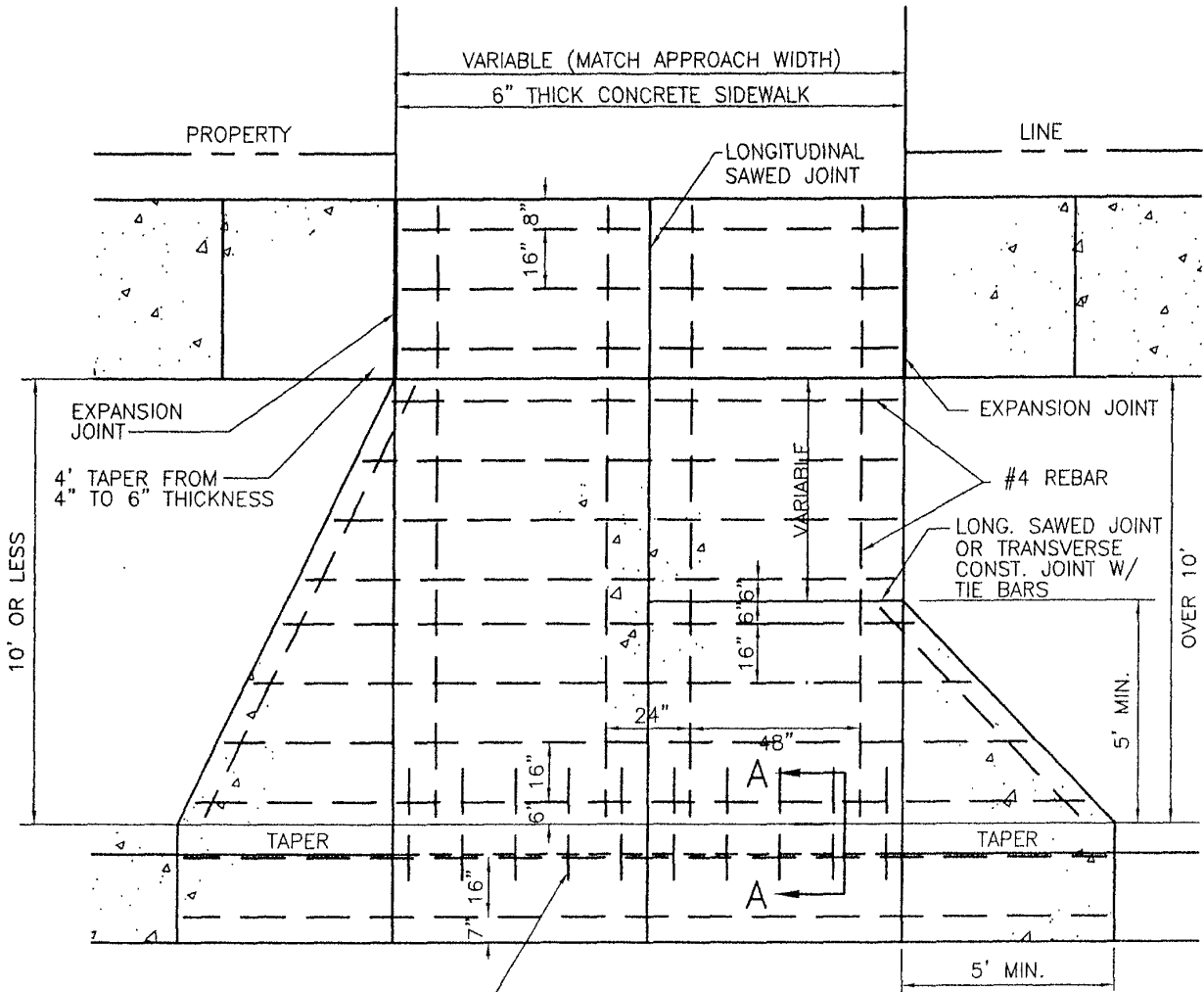


PROPERTY LINE SIDEWALK

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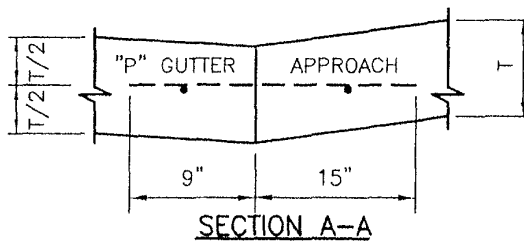
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#4 x 24" EPOXY COATED TIE BARS @ 15" O.C. TO BE PLACED BETWEEN P-GUTTER & DRIVEWAY

WHEN REMOVING EXISTING CURB & GUTTER FOR NEW APPROACH CONSTRUCTION, AN EXPANSION JOINT SHALL BE CONSTRUCTED. (SEE JOINT DETAILS 60-2 & 61-6A)

REINFORCED DRIVEWAY & SIDEWALK SHALL BE PLACED AT ALL ALLEY ENTRANCES & AT DRIVEWAYS INTO PROPERTY WHICH IS MULTI-FAMILY, COMMERCIAL, LIGHT INDUSTRIAL & HEAVY INDUSTRIAL. #4 EPOXY COATED REBAR SHALL BE PLACED AS PER DETAIL & TIED TOGETHER. TWO INCH (2") CLEARANCE SHALL BE MAINTAINED BETWEEN BOTTOM OF CONCRETE & REBAR. (PROPERTY LINE SIDEWALK SHOWN)



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REINFORCED DRIVEWAY
 APPROACH

TAPER FORMULA
 $L = S \times W$ FOR SPEEDS OF 45 OR MORE
 $L = WS/60$ FOR SPEEDS OF 40 OR LESS
 L MAY BE ADJUSTED TO FIT PROJECT CONDITIONS
 WHERE:
 L = MINIMUM LENGTH OF TAPER
 S = NUMERICAL VALUE OF POSTED SPEED LIMIT
 PRIOR TO WORK, OR AS PERCENTILE SPEED
 W = WIDTH OF OFFSET

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DRUMS OR 4' TYPE 2 BARRICADES IN TAPER
WHEN DEVICES REMAIN OVERNIGHT OR LONGER

NOTE:
THE MAXIMUM SPACING BETWEEN CHANNELIZING
DEVICES IN A TAPER SHOULD BE 30'

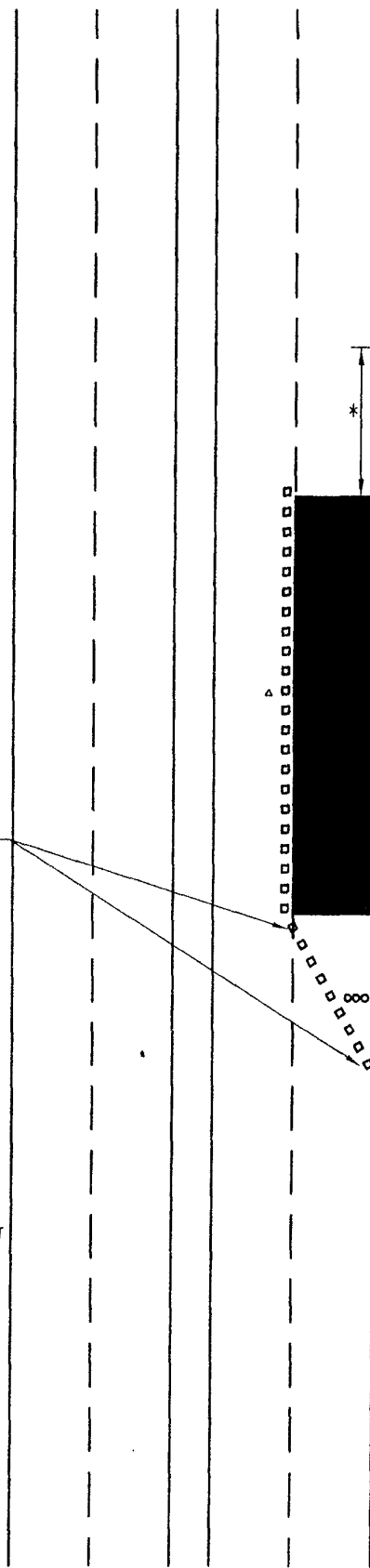
△ THE MAXIMUM SPACING BETWEEN CHANNELIZING
DEVICES IN A TAPER SHOULD BE APPROXIMATELY
EQUAL IN FEET TO THE SPEED LIMIT

□ □ CHANNELIZING DEVICES

∞∞ ARROW PANEL

* NORMAL SPACING SHOULD BE APPROXIMATELY
EQUAL TO 10 TIMES THE NUMERICAL VALUE OF
THE POSTED SPEED LIMIT PRIOR TO WORK, BUT
CAN BE ADJUSTED TO COMPENSATE FOR SIGHT
OR PROJECT CONDITIONS

*** SIGNS NOT REQUIRED IF ACTIVITY IS WITHIN
A CONSTRUCTION ZONE



END
CONSTRUCTION

G20-2
(60"x24")

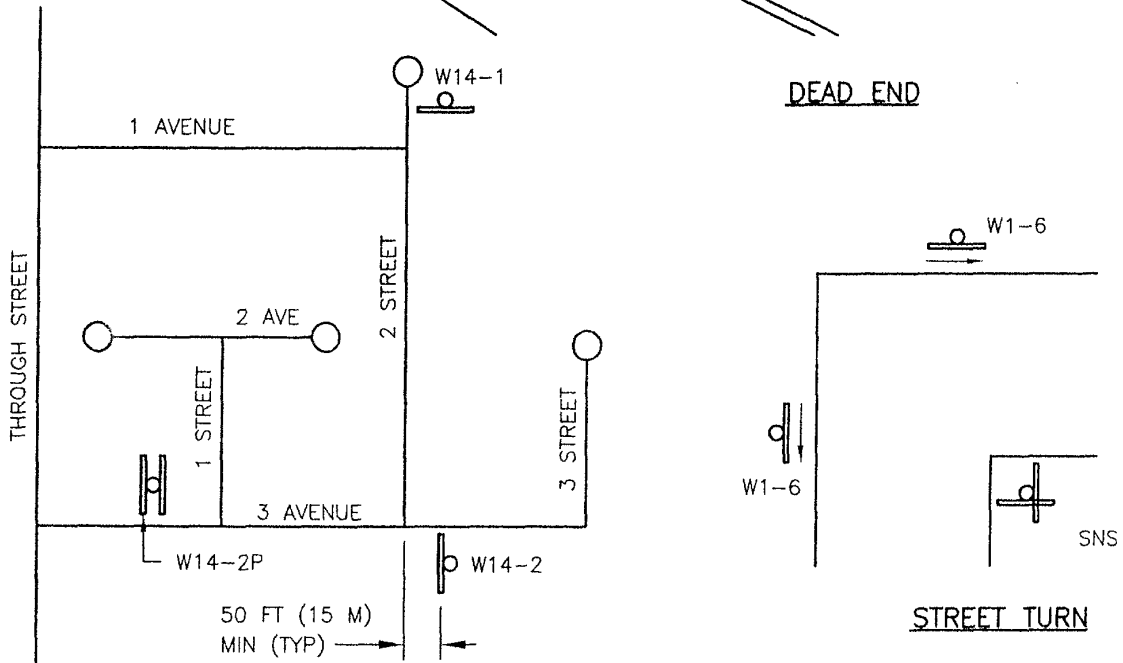
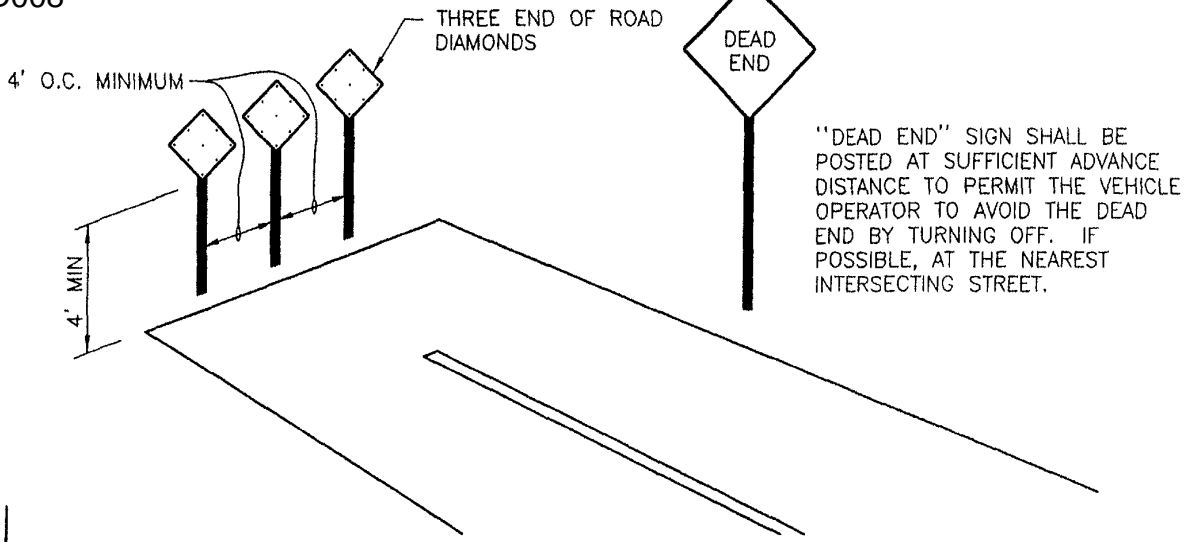
4" WHITE PAVEMENT MARKING TAPE
OR TEMPORARY ROAD MARKERS AT
5' SPACING SHALL BE INSTALLED
WHEN THE LANE IS CLOSED FOR A
PERIOD OF 24 HOURS OR MORE

L MAY BE ADJUSTED TO
FIT PROJECT CONDITIONS

W4-2
(48"x48")

W20-5
(48"x48")

W20-1
(48"x48")



TYPICAL USE OF "DEAD END" AND "NO OUTLET" SIGNS
(W14-1) (W14-2)

NOTES:

1. THIS SHEET APPLICABLE TO RESIDENTIAL AND MINOR STREETS ONLY. MAJOR STREETS ARE TO BE EVALUATED INDIVIDUALLY.
2. END OF ROAD DIAMOND IS A RED 18" REFLECTIVE SIGN WITH HIGH INTENSITY SIGN SHEETING.
3. "DEAD END" SIGNS ARE NOT REQUIRED WHERE CONDITION IS READILY EVIDENT FROM THE THROUGH STREET
4. USE W14-1P AND W14-2P SIGNS WHEN APPLICABLE ON THE STEM OF "T" INTERSECTIONS. MOUNT ON SAME POST AS STOP AND STREET NAME SIGNS.

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