



APPENDIX D

PUBLIC INVOLVEMENT

OPEN HOUSE – FEBRUARY 18, 2003

EGLIN STREET CORRIDOR STUDY

OPEN HOUSE

FEBRUARY 18, 2003

BACKGROUND

The City of Rapid City in conjunction with the Rapid City Area Metropolitan Planning Organization (MPO) and the South Dakota Department of Transportation (SDDOT) have contracted with Ferber Engineering Company and Interstate Engineering, Inc. to develop a Corridor Study for the Eglin Street Corridor from Lacrosse Street on the west to one-half mile east of Elk Vale Road.

Eglin Street currently exists as a two-lane road between East Anamosa Street and Elk Vale Road, providing primary access to Menards, the South Dakota Department of Transportation offices, the Kanab Pipeline Rapid City Terminal, and numerous industrial and commercial uses located south of I-90 between East Anamosa Street and Elk Vale Road.

The South Dakota Department of Transportation is planning the reconstruction of Exit 60 (East North Street), and Exit 61 (Elk Vale Road) in the next two to three years. Reconstruction of these interchanges will result in changes to the alignment of Eglin Street and reconfiguration of the intersection of Eglin Street and Elk Vale Road. The interchange reconstruction as well as the pending extension of Mall Drive from LaCrosse Street to Elk Vale Road will alter transportation infrastructure throughout the Study Area.

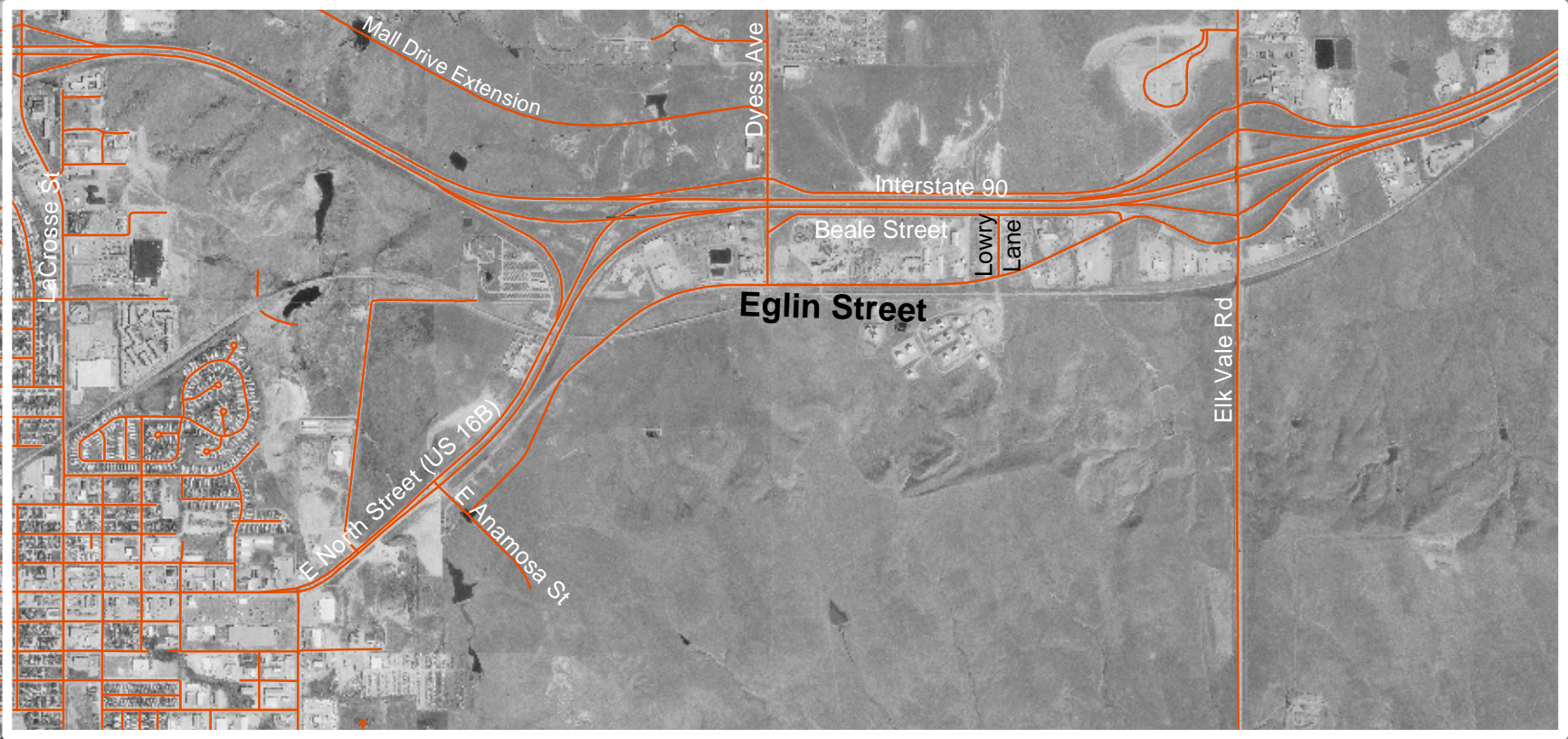
STUDY OBJECTIVES

- Identify recommendations for alternate alignments for Eglin Street
- Investigate the potential impact of these proposed changes on the level of service of existing and proposed streets and intersections
- Provide recommendations to address weaving movements related to Exit 60 and 61
- Provide recommendations regarding the traffic signal installations including phasing, signal timing, and timing coordination with other existing signals
- Provide recommendations for roadway alternative impacts on the sanitary sewer collection system
- Provide recommendations regarding Dyess Avenue and its relationship to Eglin Street
- Provide recommendations regarding the continuation of Eglin Street east of Elk Vale Road

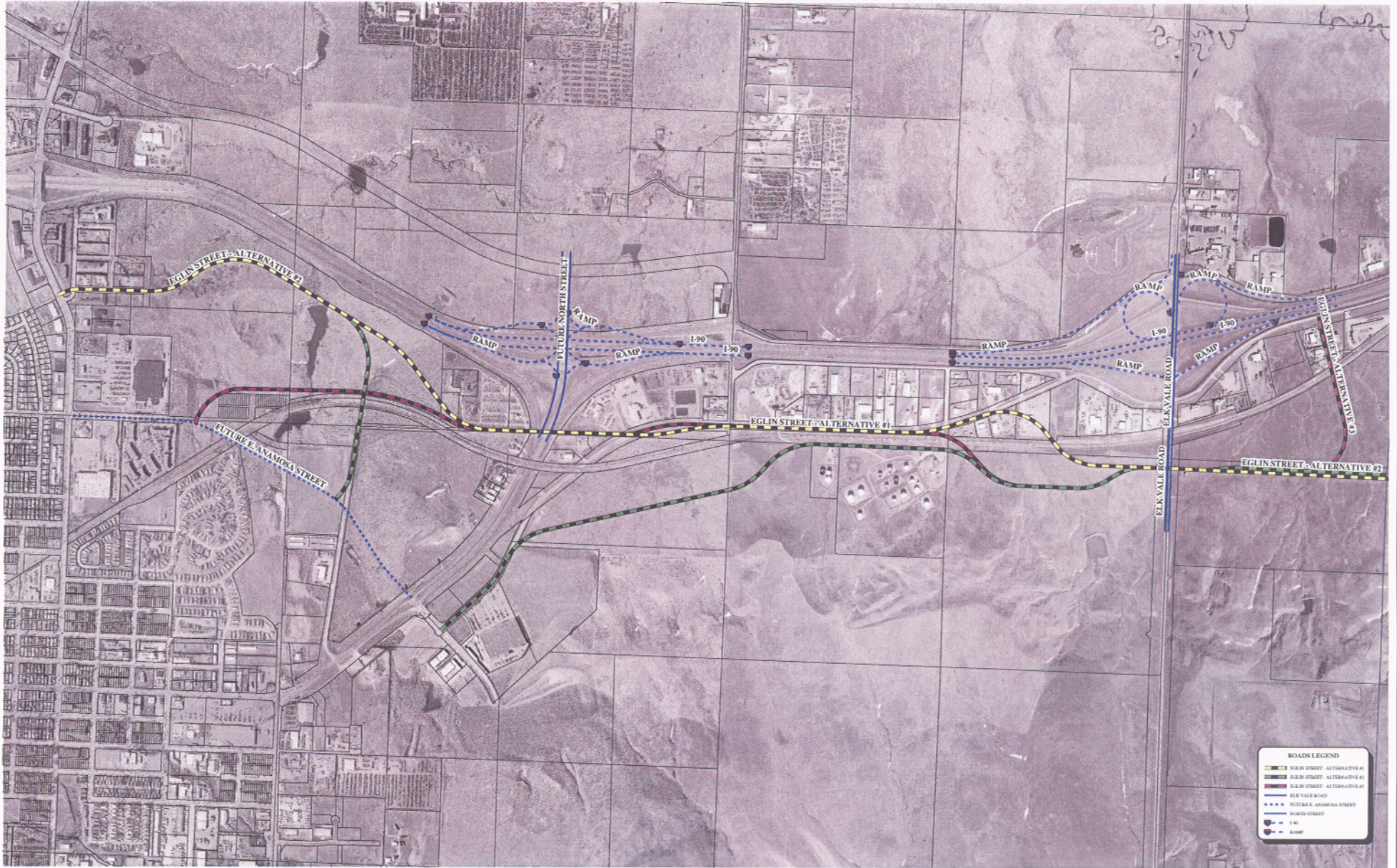
The comments and concerns of area landowners and residents are a significant part of our planning effort. We ask you to carefully review the materials presented here and share with us your comments or concerns regarding the acceptability of the various roadway alternatives, preference of one alternative over others, and the potential impact of the proposed corridor improvements on your property within the Study Area. A comment sheet has been attached to this packet for your use. Please return your comments by March 3, 2003 to Patsy Horton at the Rapid City Planning Department, 300 Sixth Street, Rapid City, SD 57701.

Eglin Street Corridor Study

Study Area Boundary

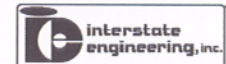


EGLIN STREET CORRIDOR STUDY PROPOSED EGLIN STREET ALIGNMENTS



ROADS LEGEND	
	EGLIN STREET - ALTERNATIVE #1
	EGLIN STREET - ALTERNATIVE #2
	EGLIN STREET - ALTERNATIVE #3
	E. K. VALE ROAD
	FUTURE E. ANAMOSA STREET
	NORTH STREET
	I-90
	I-50
	RAMP

PROJECT: EGLIN STREET CORRIDOR STUDY
 PDC JOB NO.: 002-031
 DRAWING DATE: 2-13-03
 PREPARED BY: TAMM
 AIRMAP NAME: EGLIN.ORTHO3 30X44MM2



FERBER ENGINEERING COMPANY
 CONSULTING ENGINEERS AND LAND SURVEYORS
 1411 SYDNEY ROAD
 KANSAS CITY, MO 64108
 PH: (816) 451-3111
 FAX: (816) 451-3390

EGLIN STREET CORRIDOR STUDY

OPEN HOUSE

FEBRUARY 18, 2003

RECEIVED

FEB 26 2003

Rapid City
Planning Department

Name(s): Ralph D. Newman

Address: 15756 Sidney Stage Rd. Piedmont, S.D. 57769

Phone Number: 605-287-7883

I (We) own Property within the Eglin Street Corridor Study Area Yes No

I (We) have the following comments regarding the proposed Alignment Alternatives: _____

RE: Crossing of Elk Vale Road by Eglin street. There should be a bridge built so Eglin goes under Elk-Vale - also with appropriate on/off ramps

I (We) believe that the proposed changes will have the following impact on our property: _____

"UNSURE"

4301 S. I-90 Service Rd.

Please include any additional comments you may have concerning this project. Heavy-loaded

trucks coming north ^{"downhill"} on Elk-Vale might have a hard time stopping for a light! as time goes by and traffic gets heavier it would be like putting a traffic-light on I-90.

Ferber Engineering Company
3471 Sturgis Road
Rapid City, SD 57702
(605) 343-3311

Interstate Engineering, Inc.
1505 Avenue D, Suite 210
P.O. Box 30215
Billings, MT 59107
(406) 256-1920

Patsy Horton
Rapid City Planning Dept.
300 Sixth Street
Rapid City, SD 57701
(605) 394-4120

EGLIN STREET CORRIDOR STUDY

OPEN HOUSE

FEBRUARY 18, 2003

Name(s): M.G. OIL COMPANY (TROY ERICKSON)

Address: P.O. BOX 1006 RAPID CITY SOUTH DAKOTA 57709

Phone Number: 605-342-0527

I (We) own Property within the Eglin Street Corridor Study Area Yes No

I (We) have the following comments regarding the proposed Alignment Alternatives: _____

I have 2 parcels of land that will be affected by the alignment.

1 is an ethanol plant directly across from the pipe line and

will need a crossing of Eglin street to accommodate trucks in the

ethanol fueling process. They first go to the pipe line & fill with

gasoline & then cross Eglin street to get to my plant to

blend ethanol into the gasoline then exit again on Eglin street.

I (We) believe that the proposed changes will have the following impact on our property: _____

I feel that this proposal will have little impact on my business

unless we are not given the crossing of Eglin & G150

an entrance to Eglin. I have highlighted a copy of truck

traffic & am sending it back with this letter. (X is my property.)

Please include any additional comments you may have concerning this project. _____

I also own the Flying J Travel Plaza on the North East corner

of Exit 61 and am very interested & concerned on the layout of this

exit as it will affect my business drastically. Please notify me of all meetings
so that I can participate in the initial design process. Thank you. Troy Erickson

Ferber Engineering Company
3471 Sturgis Road
Rapid City, SD 57702
(605) 343-3311

Interstate Engineering, Inc.
1505 Avenue D, Suite 210
P.O. Box 30215
Billing, MT 59107
(406) 256-1920

X Patsy Horton
Rapid City Planning Dept.
300 Sixth Street
Rapid City, SD 57701
(605) 394-4120



KANEB PIPE LINE
OPERATING
PARTNERSHIP, L.P.

February 28, 2003

Ms. Patsy Horton
Rapid City Planning Department
300 6th Street
Rapid City, SD 57701

RE: Eglin Street Corridor Study

Dear Ms. Horton:

I am writing you to follow up on discussions held at the February 18, 2003 open house for the above referenced project. We had our local representative, Mr. Ron Kleinsasser, present for the meeting. He has forwarded information regarding the project study to this office for our review and comment.

As you are aware, Kaneb owns and operates a petroleum pipeline terminal and truck loading facility along the Eglin corridor. Kaneb has a high volume of transport trucks moving in and out of this facility. Our hours of operation are 24-hours a day, 365 days a year, so we are very interested in the project study due to safety considerations to our customers, the general public, as well the impacts to our facility operations.

Of the three options that have been presented to date, Kaneb would be most in favor of the option exhibited with the yellow/black line. This would provide the least amount of interruption or impact to our terminal operations and to the traffic flow in and out of our facility. It appears that this route would not disrupt any of our existing terminal property, and would also not change the location or alignment of our existing access driveway to our terminal.

It appears that the purple/black option would require the relocation of our terminal access driveway. Due to the existing location of our truck loading docks, terrain and traffic flow moving our driveway to another location would not be practical. Relocating of the loading docks would not only be impractical, but would have a cost in excess of 2 million dollars. Additionally, the traffic flow from our load docks, easterly to the proposed route of Eglin, would cause all transport truck traffic to enter and exit Eglin on a curve causing a great deal of concern with traffic and public safety.

The green/black route would be the least desirable to Kaneb. This route would require the acquisition of land along Kaneb's north property line, which, along with the railroad

7340 West 21st St. N., Suite 200
Wichita, Kansas 67205
phone 316.773.9000
fax 316.773.9001
www.kanebpipeline.com

right-of-way, provides Kaneb both a visual and security buffer to the general public. This route would also impact existing water collection facilities, as well as ground water monitoring wells in the area. Kaneb would be required to relocate and re-permit these facilities to other areas. In addition, this would require the relocation of our terminal access driveway as it is with the previous option, thus with the same impacts to Kaneb.

Of the options proposed to date, Kaneb is most desirous of the yellow/black route. This would allow us to maintain our existing driveway location, as well as allow us to maintain our existing level of terminal security, and not have any impacts to the current level of traffic safety. We appreciate the opportunity to provide comment to this matter and appreciate the consideration to our comments. Feel free to contact this office if we can answer any additional questions, or provide you additional information, regarding this matter.

Sincerely,

A handwritten signature in black ink, appearing to read 'Dale E. Smith', written in a cursive style.

Dale E. Smith
Manager of Engineering

CC: Mr. Ron Kleinsasser, Kaneb Pipe Line Company
Rapid City, South Dakota

EGLIN STREET CORRIDOR STUDY

OPEN HOUSE

FEBRUARY 18, 2003

Name(s): Wartenbee Family Limited Partnership

Address: PO Box 1104 Sioux Falls, SD 57101-1104

Phone Number: 605-543-5206

I (We) own Property within the Eglin Street Corridor Study Area Yes No

I (We) have the following comments regarding the proposed Alignment Alternatives: We own property at 3505 Eglin Street, which is located between Lowry Lane and Elk Vale Road. Under the Proposed Alternative #1 Route, it would not only divide our property, but pass thru our existing building structure. Which would create an undo hardship of 13+ years of established business

I (We) believe that the proposed changes will have the following impact on our property: _____

Under the Proposed Alternate #1, it would require relocating our entire business. There by impacting our established business for many years to come.

Please include any additional comments you may have concerning this project. We recently

completed several improvements to our property including a 2400 sq foot office & parts showroom. During the building permit application, we were

(over)

Ferber Engineering Company
3471 Sturgis Road
Rapid City, SD 57702
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P.O. Box 30215
Billings, MT 59107
(406) 256-1920

Patsy Horton
Rapid City Planning Dept.
300 Sixth Street
Rapid City, SD 57701
(605) 394-4120

never informed about the proposal, which would have impacted our decision to expand in that area. There is a vacant lot on the east side of our property that would make a better proposed route and be more cost effective or it would appear if the railroad line was moved south, the proposed Eglin Route would be less costly and could run straight. This would create a better appearance for the property owners on the north side and creating a better exposure for the businesses located along there. It would be more accessible and a more user friendly access. Or why change Eglin Street - Why not put a cul-de-sac at Elk Vale and then put a whole new street on the south side of the properties, with two streets running north to hook up to Eglin Street

Please keep us informed of any and all future developments.



LEWIS-KIRKBY-HALL
REAL ESTATE, INC.

2700 WEST MAIN STREET
RAPID CITY, SD 57702
BUS. (605) 343-2700
TOLL FREE (888) 343-2710

March 5, 2003

RECEIVED

MAR 10 2003

Rapid City
Planning Department

Patsy Horton
Transportation Planning Coordinator
300 6th Street
Rapid City, SD 57701

Dear Ms. Horton:

Thank you for your positive marketing on Eglin Street.

I am representing Duane Pankratz on behalf of the land that he owns that is located at the corner of Eglin's proposed new road and Elk Vale Road.

Dr. Pankratz would like Eglin Street to enter his land which is east of the Kanab Pipeline and adjoins that property on the purple line (alternative #3) used on proposed Eglin Street alignments. We would like to meet with the city and state parties so as not to adversely affect his land as the road crosses to Elk Vale. We want to work within your needs.

Going west from our project and property we would hope you would see that alternative #1 is the best plan.

The impact on Dr. Pankratz property can be severe when Eglin crosses to Elk Vale. We would request that Dream Design International can visit with your engineers to address drainage and grading concerns of all land impacted.

Thank you for your time and sensibility to this project.

Please contact me at 484-5000 regarding all the steps of progress.

Sincerely,

Patrick R. Hall

PRH/ek



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RECEIVED

EGLIN STREET CORRIDOR STUDY

OPEN HOUSE

FEBRUARY 18, 2003

MAR 11 2003

Rapid City
Planning Department

Name: Andrew Vig (Wood Stock Supply Inc)

Address: 4705 South - I-90 Service Road

Phone Number: 605-341-6900

I (We) own Property within the Eglin Street Corridor Study Area Yes No

I (We) have the following comments regarding the proposed Alignment Alternatives: _____

My greatest concern is that we
continue to have left in and left out
on the South - I-90 Service Road to
Elk Vale Road. In my opinion a
urban interchange similar to the Hawes
Ave might work very well.

I (We) believe that the proposed changes will have the following impact on our property: _____

Not having left in and left out would
be a great hardship to our business.
~~Our~~ Our customers and delivery trucks need as
straight of shot travel wise as possible.

Please include any additional comments you may have concerning this project. It would
be my hope that there would be great
consideration paid to the existing businesses
and how these changes impact them.

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Rapid City, SD 57702
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Rapid City Planning Dept.
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EGLIN STREET CORRIDOR STUDY

OPEN HOUSE

FEBRUARY 18, 2003

Name(s): Dorene Talley

Address: 2830 Eglin St

Phone Number: 342-8649

I (We) own Property within the Eglin Street Corridor Study Area Yes No

I (We) have the following comments regarding the proposed Alignment Alternatives: Alternative 2 will increase the amount of traffic having to cross the railroad tracks - Trucks & passengers - I do not like. I do not like the east end of alternative #3 as this will take away a substantial amount of outdoor area for my cattle equipment at CowCountry & make it impossible for trucks to get in front of my property.

I (We) believe that the proposed changes will have the following impact on our property: _____

Alternative #2 will force my customers to cross the railroad tracks with load of recyclables & steel & so forth. In the summertime that can be as many as 200 people a day. This may turn business away

Please include any additional comments you may have concerning this project. I do not like

where Eglin crosses Elk Vale - you have it at the bottom of a hill & with the truck traffic - that makes it a very dangerous situation. I believe

Eglin St. is fine as it is but put a traffic light at the Eglin Elk Vale Intersection - only if it would eliminate a lot of problems

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300 Sixth Street
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If you could make Eglin go under Elk Vale - like the railroad tracks that would solve a lot of problems - I know

EGLIN STREET CORRIDOR STUDY

OPEN HOUSE

FEBRUARY 18, 2003

RECEIVED

MAR 10 2003

Rapid City
Planning Department

Name(s): FRANK BOYLE / Dealer Exchange

Address: 6212 Sunridge Rd RC 57702

Phone Number: 390-6220 347-0052

I (We) own Property within the Eglin Street Corridor Study Area Yes No

I (We) have the following comments regarding the proposed Alignment Alternatives: _____

If Beak St. service road turns into a
Cul-de-sac at Elk Vale Rd. - it'll have
a definite deffecent effect - After that
the yellow proposed alignment needs to
access Eglin as close as possible
KN subdivision lot 2 of Lot D & lot 3 of Lot D

I (We) believe that the proposed changes will have the following impact on our property: _____

Please include any additional comments you may have concerning this project. I've been
paying taxes + waiting for city water for 8 years -
now have water coming but worried ^{now} about access
to have full potential for property. Thanks JB

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Response to comments from Open House February 18, 2003

Comment: “There should be a bridge built so Eglin goes under Elk Vale – also with appropriate on/off ramps.”

Response: In this area there already exists a grade-separated rail crossing and I-90 Exit 61, which will be reconstructed in 2007. There is physically not enough room between the interstate and the railroad tracks develop another grade-separated interchange of Elk Vale road.

Comment: “Heavy loaded trucks coming north on Elk-Vale might have a hard time stopping for a light!”

Response: This comment deals with the intersection between Elk Vale Road and Eglin Street / Cheyenne Boulevard. It was made by a number of people who made written comments following the first public meeting. The SDDOT established this intersection location by looking at two main things. The first being the slope coming down the hill as Elk Vale heads north towards I-90. The second was the distance from Exit 61. This intersection will be signalized in the future and this location provides adequate spacing from Exit 61 without forcing the intersection farther to the south and up the hill on a steeper slope.

Comment: “...trucks in the ethanol fueling process...first go to the pipeline and fill with gasoline and then cross Eglin Street to get to my plant to blend Ethanol into the gasoline then exit again on Eglin Street. I feel that this proposal will have little impact on my business unless we are not given the crossing of Eglin and also an entrance to Eglin.”

Response: The recommended alternative detail shown in Figure 5-5 shows how Eglin Street will cross the railroad tracks and how access to the Kaneb Pipeline terminal will be accomplished. The Eglin Street crossing will replace the existing Kaneb Pipeline access.

Comment: “... the Flying J Travel Plaza on the northeast corner of Exit 61 and am very interested and concerned on the layout of this exit as it will affect my business directly.”

Response: The SDDOT is currently investigating options for the reconstruction of this Interchange.

Comment: “It appears that the purple/black option would require the relocation of our (Kanab Pipeline) terminal access driveway. ...Additionally, the traffic flow from our load docks, easterly to the proposed route of Eglin on a curve causing a great deal of concern with traffic and public safety. The green/black route would be the least desirable to Kanab. This route would require the acquisition of land along Kanab’s north property line, which along with the railroad right-of-way, provides Kanab both a visual and security buffer to the general public.”

Response: Figure 5-5 shows our recommendations for the relocated Kanab access. The access will tie to Eglin Street perpendicularly and the location it ties to the existing Kanab property can shift to the north or south to better accommodate traffic flow patterns within the terminal. The intersection between the access and Eglin Street is shown at a location where there is good sight distance in both directions along Eglin.

Alternative #2 which ran between the DM&E railroad tracks and the pipeline was eliminated from consideration in part due to the right-of-way and safety concerns that affected the Kanab property.

Comment: Under the Proposed Alternative #1 Route, it would not only divide our property, but pass through our existing building structure.”

Response: The design speed of the curves near the Lowry Lane intersection and the DM&E railroad crossing was lowered to 30 mph between the first and second open houses to address this comment as well as the concern for right-of-way acquisition. Revised alternatives #1 and #3 on Figure 4-2 and the recommended alignment shown on Figure 5-1 reflect these changes and will not require the demolition of any existing structures.

Comment: “Dr. Pankratz would like Eglin Street to enter his land which is east of the Kanab pipeline and adjoins that property on the purple line (alternative #3)...we request that Dream Design International can visit with your engineers to address drainage and grading concerns of all land impacted.”

Response: The recommended alternative as presented in the Study will enter Dr. Pankratz property along the Alternative #3 alignment. Dr. Pankratz and/or his representatives are welcome to attend the public presentations. At such time as the City of Rapid City or the SDDOT proceeds to design the facilities across his property, further discussion will be appropriate.

Comment: “...greatest concern is that we continue to have left in and left out on the South I-90 Service Road to Elk Vale Road.”

Response: This comment was made by a number of landowners concerned about the South I-90 Service Road (Eglin Street) / Elk Vale Road intersection being limited to right in right out access only. The traffic analysis performed as a part of this study shows that under future traffic conditions if this intersection

were to remain full movement it would need to be signalized. Due to the proximity of the this intersection to Exit 61, and in an effort to reduce the number of signalized intersections on this “system” road, installation of a traffic signal at this location is not prudent under existing intersection spacing conditions along Elk Vale Road. To mitigate the loss of easy access, a connection from Eglin Street to Elk Vale Road has been proposed further south, across the DM&E railroad tracks at the Cheyenne Boulevard intersection location.

Comment: “I do not like the east end of Alternative #3 – as this will take away a substantial amount of outdoor area for my cattle equipment storage at Cow Country and make it impossible for trucks to get in and out of my property.”

Response This comment deals with the connection of Degeest Drive across the railroad tracks to the South I-90 frontage road. The final alignment of this connection will be determined in the future during final design. At that time right-of-way concerns and access issues can be addressed along Degeest.

EGLIN STREET CORRIDOR STUDY

OPEN HOUSE

FEBRUARY 18, 2003

Name(s): _____

Address: _____

Phone Number: _____

I (We) own Property within the Eglin Street Corridor Study Area Yes _____ No _____

I (We) have the following comments regarding the proposed Alignment Alternatives: _____

I (We) believe that the proposed changes will have the following impact on our property: _____

Please include any additional comments you may have concerning this project. _____

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Patsy Horton
Rapid City Planning Dept.
300 Sixth Street
Rapid City, SD 57701
(605) 394-4120

OPEN HOUSE – APRIL 22, 2003

EGLIN STREET CORRIDOR STUDY

OPEN HOUSE

April 22, 2003

BACKGROUND

The City of Rapid City in conjunction with the Rapid City Area Metropolitan Planning Organization (MPO) and the South Dakota Department of Transportation (SDDOT) have contracted with Ferber Engineering Company and Interstate Engineering, Inc. to develop a Corridor Study for the Eglin Street Corridor from Lacrosse Street on the west to one-half mile east of Elk Vale Road.

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The South Dakota Department of Transportation is planning the reconstruction of Exit 60 (East North Street), and Exit 61 (Elk Vale Road) in the next two to three years. Reconstruction of these interchanges will result in changes to the alignment of Eglin Street and reconfiguration of the intersection of Eglin Street and Elk Vale Road. The interchange reconstruction as well as the pending extension of Mall Drive from LaCrosse Street to Elk Vale Road will alter transportation infrastructure throughout the Study Area.

On February 18, 2003, an Open House was conducted to encourage property owners to comment on the information available at the Open House. The current proposed alignments incorporate comments received from the February 18, 2003 Open House into the revised alignments.

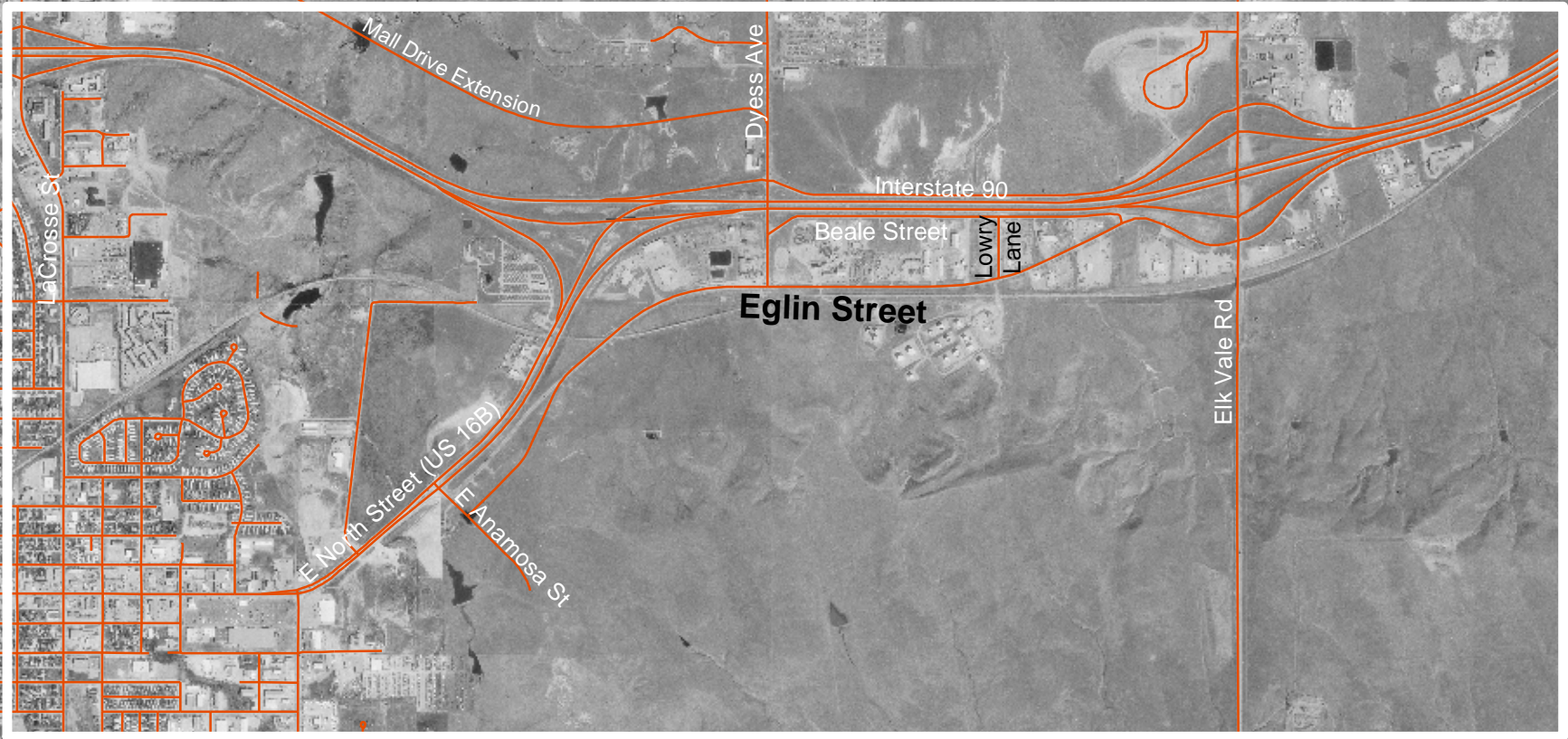
STUDY OBJECTIVES

- Identify recommendations for alternate alignments for Eglin Street
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- Provide recommendations to address weaving movements related to Exit 60 and 61
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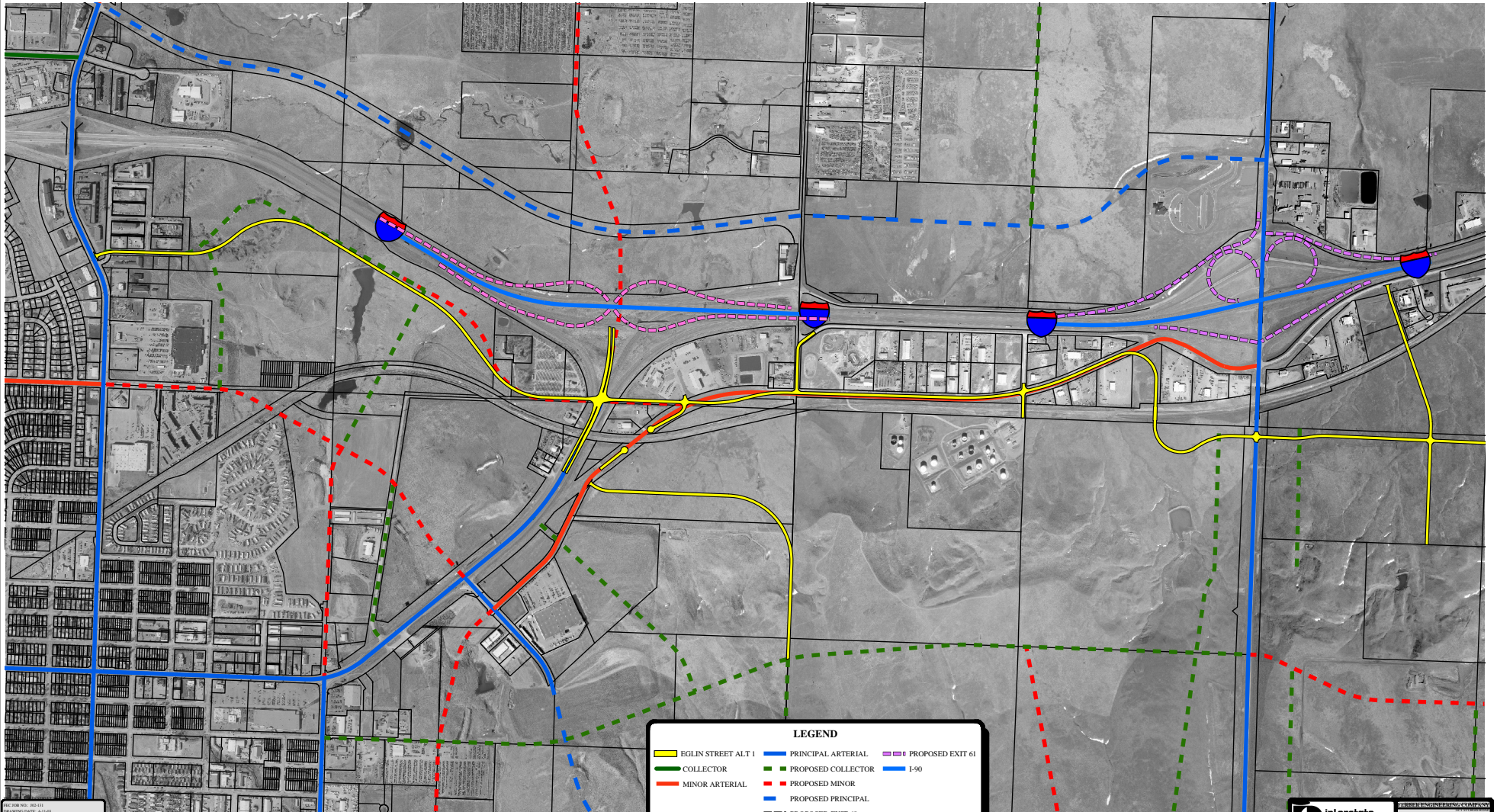
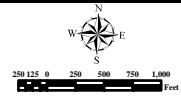
The comments and concerns of area landowners and residents are a significant part of our planning effort. We ask you to carefully review the revised materials presented here and share with us your comments or concerns regarding the acceptability of the various revised roadway alternatives, preference of one alternative over others, and the potential impact of the proposed corridor improvements on your property within the Study Area. A comment sheet has been attached to this packet for your use. Please return your comments by May 2, 2003 to Patsy Horton at the Rapid City Planning Department, 300 Sixth Street, Rapid City, SD 57701.

Eglin Street Corridor Study

Study Area Boundary



**EGLIN STREET CORRIDOR STUDY
ALTERNATIVE #1 ALIGNMENT**

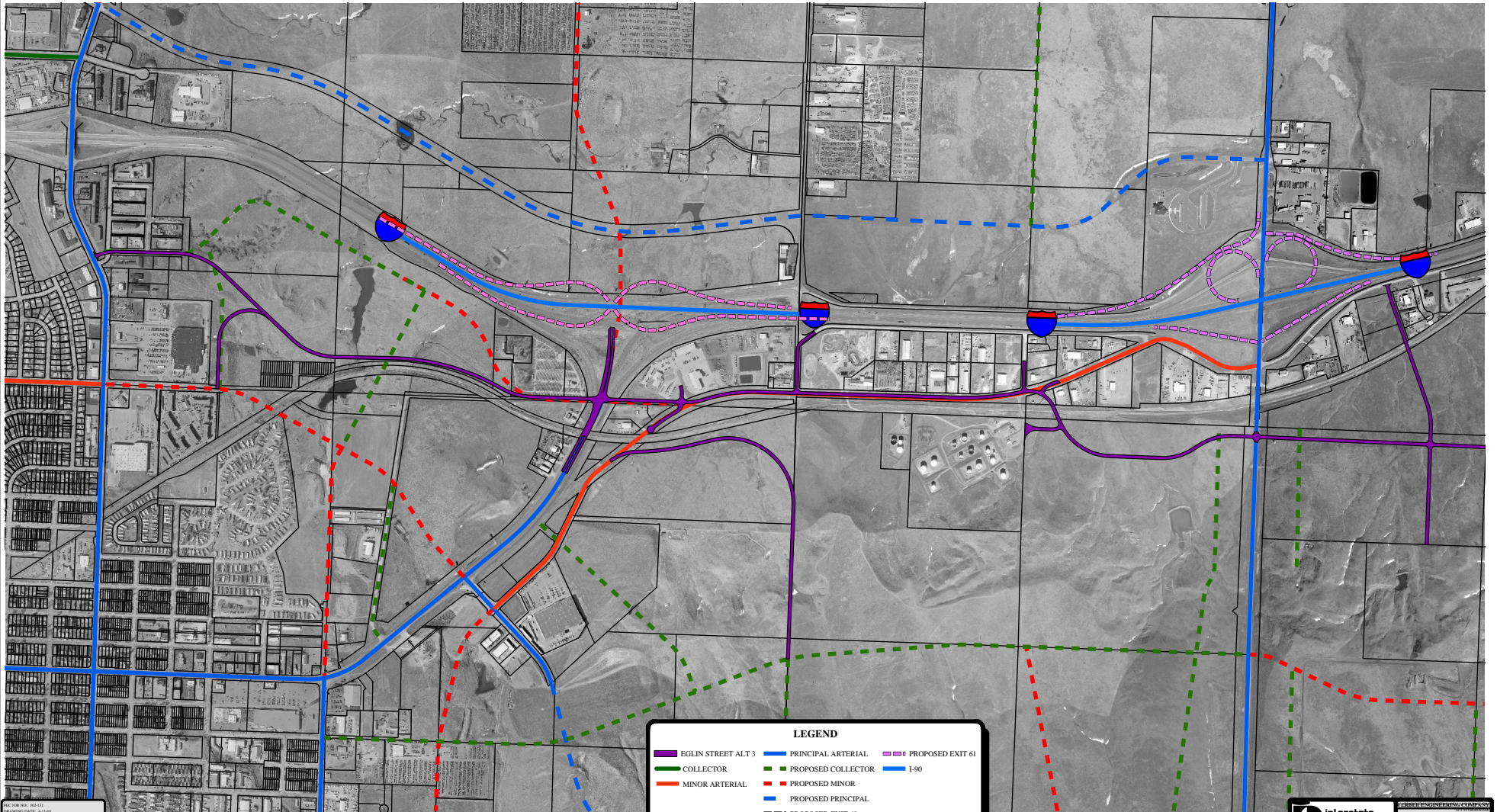
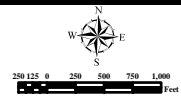


LEGEND					
	EGLIN STREET ALT 1		PRINCIPAL ARTERIAL		PROPOSED EXIT 61
	COLLECTOR		PROPOSED COLLECTOR		I-90
	MINOR ARTERIAL		PROPOSED MINOR		PROPOSED PRINCIPAL
			PROPOSED EXIT 60		

DATE: 08/14/2013
 DRAWING DATE: 08/14/2013
 PROJECT NO: 13084
 ARCH/APP NAME: EGLIN ALTERNATIVE 01-01-002

INTERSTATE ENGINEERING, COMPANY
 4400 W. 10TH STREET
 SUITE 1000
 DENVER, CO 80202

**EGLIN STREET CORRIDOR STUDY
ALTERNATIVE #3 ALIGNMENT**

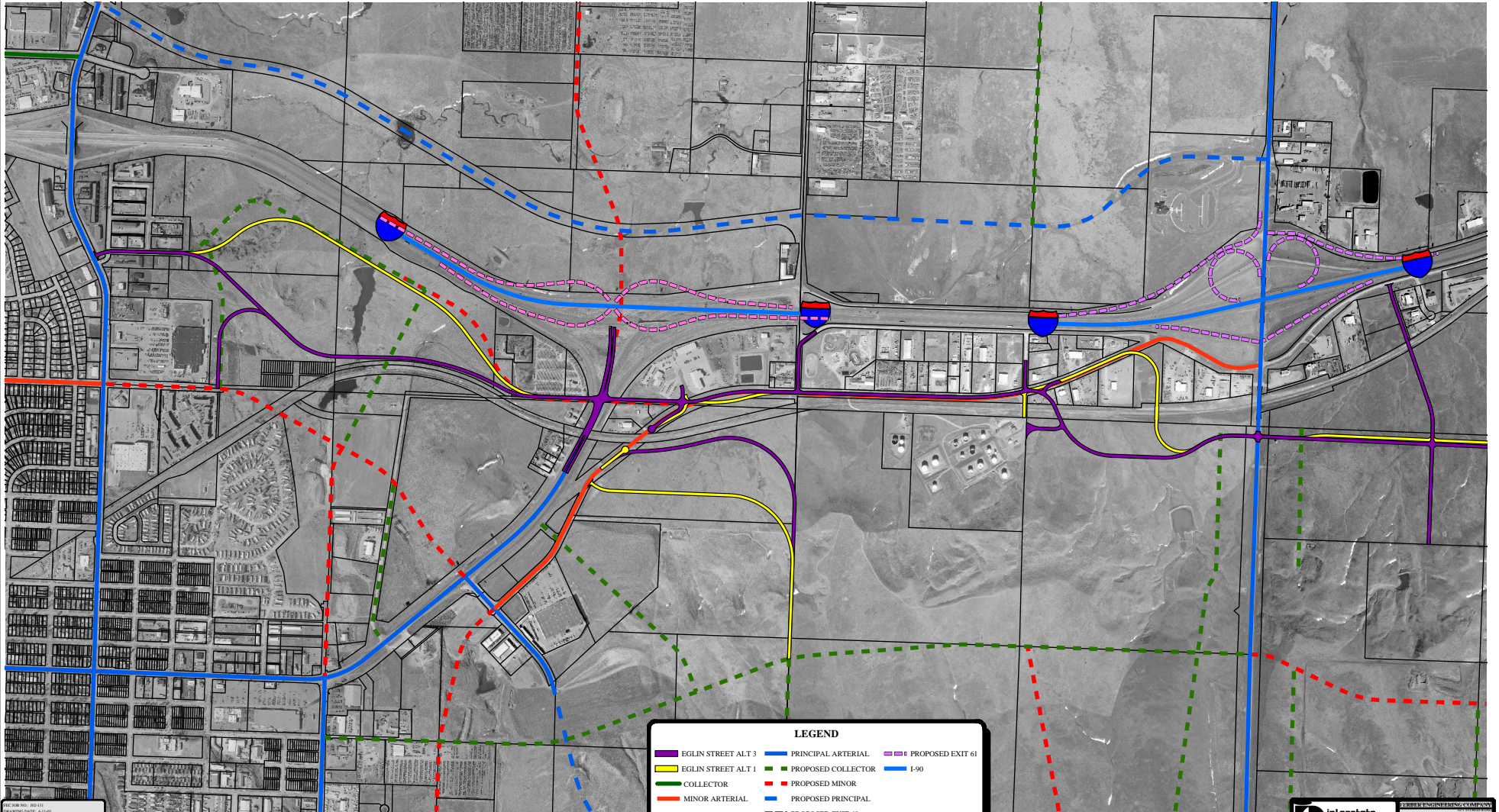
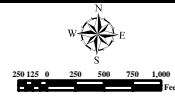


LEGEND					
	EGLIN STREET ALT 3		PRINCIPAL ARTERIAL		PROPOSED EXIT 61
	COLLECTOR		PROPOSED COLLECTOR		I-90
	MINOR ARTERIAL		PROPOSED MINOR		PROPOSED PRINCIPAL
			PROPOSED PRINCIPAL		PROPOSED EXIT 60

DATE: 08/11/2011
 DRAWING DATE: 08/11/2011
 PROJECT: EGLIN STREET CORRIDOR STUDY
 ARCH/APP NAME: EGLIN ALTERNATIVE #3 (11/11/2011)

INTERSTATE ENGINEERING, COMPANY
 4400 S. 10TH AVENUE, SUITE 100
 DENVER, COLORADO 80202
 PHONE: 303.755.1100
 FAX: 303.755.1101

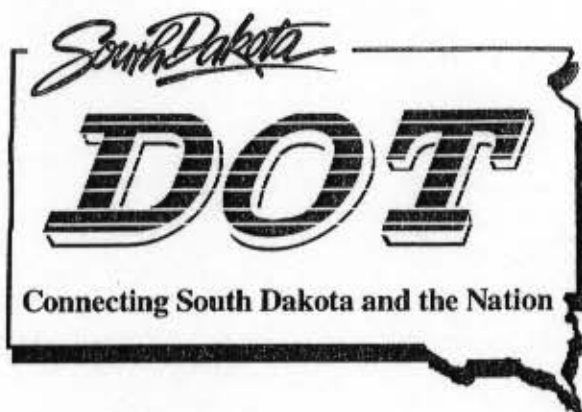
**EGLIN STREET CORRIDOR STUDY
ALTERNATIVES #1 AND #3 ALIGNMENT**



LEGEND					
	EGLIN STREET ALT 3		PRINCIPAL ARTERIAL		PROPOSED EXIT 61
	EGLIN STREET ALT 1		PROPOSED COLLECTOR		I-90
	COLLECTOR		PROPOSED MINOR		I-90
	MINOR ARTERIAL		PROPOSED PRINCIPAL		PROPOSED EXIT 60

DATE: 08/11/2011
 DRAWING DATE: 08/11/2011
 PROJECT: EGLIN STREET CORRIDOR STUDY
 ARCH/APP NAME: EGLIN ALTERNATIVE 01-01-001

interstate engineering, inc.
 11000 W. CENTRAL EXPRESSWAY
 SUITE 1000
 DENVER, CO 80231
 TEL: 303.755.1100
 FAX: 303.755.1101



**Public Hearing/
Open House**

April 22, 2003

PROJECTS IM 90-2(134)59 PCEMS 4259

&

P 2016(13)71 PCEMS 6227

PENNINGTON COUNTY

I-90 Exit 60

**Construct New Interchange @ E. North Street
Reconstruct Mainline Interstate
Reconstruct US16B from the RR Tracks North to Exit 60**

PROJECT IM-NH 90-2(39)61 PCEMS 1939

PENNINGTON COUNTY

Interstate 90 – Exit 61

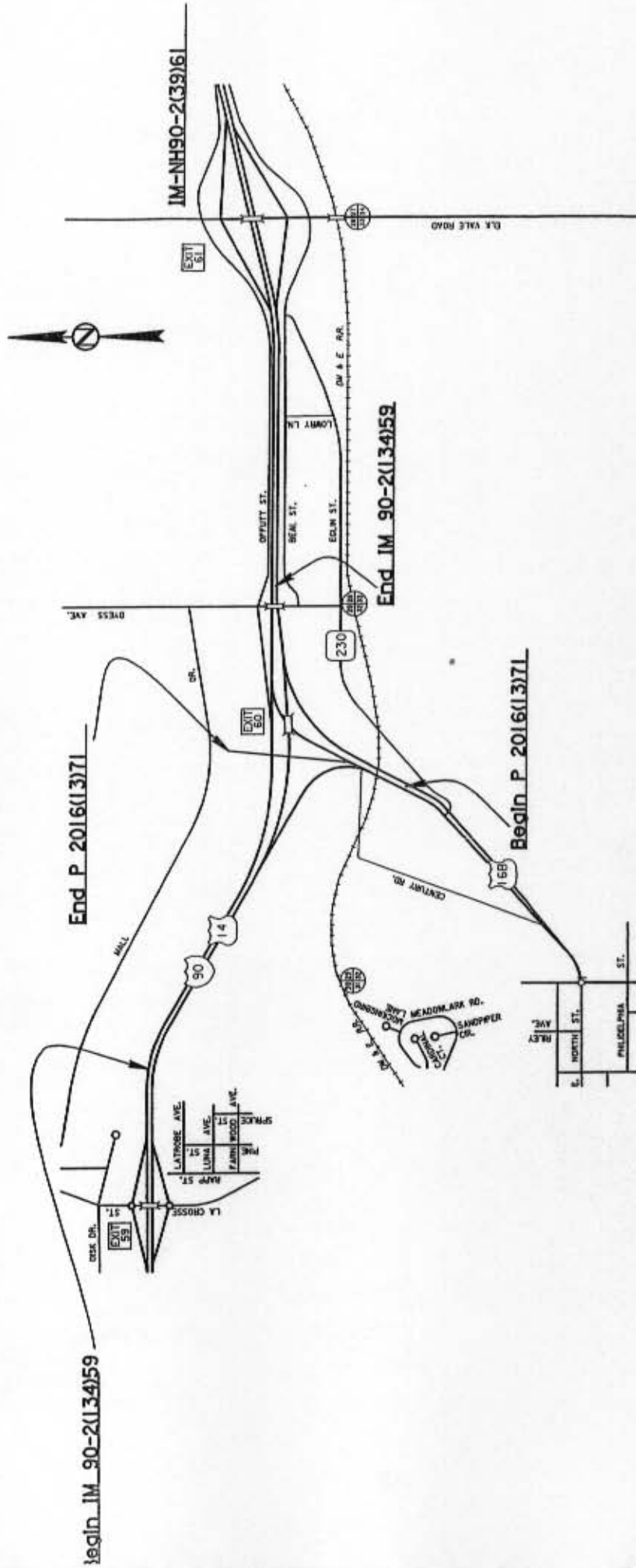
**Elk Vale Road, from North of the RR tracks to North of Mall Drive
Replace Structure over I-90
Future Interchange Design at Exit 61**

The South Dakota Department of Transportation provides services without regard to race, color, gender, religion, national origin, age or disability, according to the provisions contained in SDCL20-13, Title VI of the Civil Rights Act of 1964, the Rehabilitation Act of 1973, as amended, the Americans With Disabilities Act of 1990 and Executive Order 12898, Federal Actions to Address Environmental Justice in Minority Populations and Low-Income Populations, 1994 .

STATE OF SOUTH DAKOTA
DEPARTMENT OF TRANSPORTATION
PLANS FOR PROPOSED

PROJECT NOS. IM 90-2(134)59,
P 2016(13)71 & IM-NH90-2(39)61
I90 (EXIT 60, EXIT 61) & US16B
PENNINGTON COUNTY

PCEMS NO. 4259, 6227 & 1939



**IM 90-2(134)59 PENNINGTON COUNTY PCEMS 4259
P 2016(13)71 PCEMS 6227**

Interstate 90 – Exit 60

Construct New Interchange @ E. North Street, Reconstruct Mainline Interstate
Reconstruct US16B from the RR tracks North to Exit 60

Existing

- Originally constructed in 1960
- High accident location mostly occurring during slippery roadway conditions
- No existing north connection to the interchange
- No westbound on-ramp at the existing interchange
- Sight distance problem on westbound exit @ Dyess Ave.
- Current average daily traffic (ADT):
Year 2001: 11,720 Interstate
1,545 EB Offramp
3,900 EB Onramp
3,565 WB Offramp
2,630 WB Onramp
- Projected ADT:
Year 2021: 21,305 Interstate
2,810 EB Offramp
7,090 EB Onramp
6,480 WB Offramp
4,780 WB Onramp

Proposed Project

- Three options for Exit 60 Interchange (see following pages for layouts)
- All interchange options will have the following in common:
 - New Interchange (Exit 60)
 - Provide north connection to the Interstate
 - Provide westbound connection to the Interstate
 - Interstate overhead of US16B
 - Portland cement concrete surfacing
 - Anticipated grading & surfacing: 2005 (depending on availability of federal funds)
- Estimated cost for project \$11 million

Option 1 - Red Alignment

Single Point Interchange

Advantages:

- One signal system
- Least amount of right-of-way acquisition

Disadvantages:

- Not a typical interchange configuration
- Estimated Cost: \$9.5 million

Option 2 - Pink Alignment
Standard Diamond Interchange

Advantages:

- Estimated Cost: \$8.6 million

Disadvantages:

- Two signal systems required
- Right-of-way acquisition

Option 3 - Blue Alignment
Partial Cloverleaf Interchange

Advantages:

- Safer weaving of traffic with separated road configuration on I-90

Disadvantages:

- Large amount of right-of-way acquisition
- Estimated Cost: \$11.1 million

IM-NH 90-2(39)61 PENNINGTON COUNTY PCEMS 1939

Interstate 90 – Exit 61

Elk Vale Road from N. of the RR tracks to N. of Mall Drive
Replace Structure over I-90
Future Interchange Design @ Exit 61

Existing

- Originally constructed in 1960
- 2 - 12' lanes on Elk Vale Rd.
- Current average daily traffic (ADT):
Year 2001 23,195 Interstate
2,735 EB Offramp
4,055 EB Onramp
3,985 WB Offramp
3,280 WB Onramp
- Projected ADT:
Year 2021 42,175 Interstate
4,975 EB Offramp
7,370 EB Onramp
7,245 WB Offramp
5,965 WB Onramp

Proposed Project

- New structure over interstate will allow for grade raise & widening Elk Vale Rd. to 4 - 12' traffic lanes
- Grade approximately 500' of each ramp to match raised grade of Elk Vale Rd.
- Realign service road north of I-90
- Accommodate for future Exit 61 interchange, which will be built when funds become available (See layouts on following pages)
- Portland cement concrete surfacing
- Raised median from RR tracks to Mall Drive.
- Anticipated grading & surfacing: 2006 (depending on availability of federal funds)
- Estimated cost: \$5.4 million.

**IM 90-2(134)59 PCEMS 4259
P 2016(13)71 PCEMS 6227
&
IM-NH 90-2(39)61 PCEMS 1939
I-90 (Exit 60 & Exit 61) & US16B
Pennington County**

Environmental, Social & Economic Impacts

- This project will comply with all State and Federal environmental regulations.
- No splitting of neighborhoods is anticipated.

SDDOT Right of Way Information

IM 90-2(134)59 PCEMS 4259
P 2016(13)71 PCEMS 6227
IM-NH 90-2(39)61 PCEMS 1939
PENNINGTON COUNTY

Individual Landowner Meetings: During the early stages of the project's design, SDDOT will schedule a meeting with landowners living adjacent to the project. See the following page for an explanation of this meeting.

Property Acquisition Offer: After the project construction plans have been prepared and the right of way limits have been established, your property will be appraised to determine the fair market value of that portion of your property that is needed for construction of the project. The amount established is the basis for the offer that the Right of Way Agent will make to you. If you feel that you cannot accept the State's offer, you have the right to have the amount of just compensation established by the courts.

Relocation Assistance Program: This program provides a variety of services and payments to owners and tenants who have personal property affected by the right of way being acquired for the project.

Relocation payments are in addition to payments made for the real property being acquired. So as not to jeopardize your eligibility for payments, do not do anything until you have received a written relocation offer or have contacted Clayton Sonnenschein of the SDDOT Right of Way Program in Pierre. His phone number is 773-3746. Anyone not satisfied with the relocation offer made to them may appeal using the procedures described in the Relocation Brochure.

The landowner may also be reimbursed for various fair and reasonable incidental expenses that may be incurred during the transfer of property to the State.

Right of Way Information Brochures: Two brochures have been prepared which explain the SDDOT's Right of Way process. They provide in-depth information on your rights with regard to the acquisition of your property and the benefits available to you with regard to the Relocation Assistance Program. These brochures are available at this hearing on the "Sign-in" table. Please feel free to take a copy of each with you.

Individual Landowner Meetings

Individual landowner meetings will be scheduled at a later date. You will be contacted to set up the time and place. The purpose of this meeting is to provide you with an opportunity to comment on various issues pertaining to the design of this highway project as it relates to your property.

The following topics will be discussed at the meetings. Please note that not all topics will apply to every property owner.

1. Permanent purchase and/or temporary use of your property.
2. Locations and widths of entrances to your property: The standard South Dakota Department of Transportation (SDDOT) entrance-width for rural highways is 24 feet. Note: In general, existing entrance widths along rural State Highways are 24 feet or smaller. A maximum width of 40 feet is allowed at locations where it is deemed appropriate and necessary. Entrances in urban areas can vary from 16 feet to 40 feet.

The goal of the SDDOT is to provide property owners located adjacent to the project with the access they need, and at the same time, enhance highway safety and reduce project costs. In some instances, the SDDOT may seek to combine duplicate entrances. For example, if your property has two or three entrances to the same property that are located close to each other, we would ask you to assess your current entrance needs and consider one entrance location that will meet those needs.

3. Permanent fencing adjacent to the highway: SDDOT's fencing policy allows for the replacement of all disturbed fence with like-kind fence.

Two fence types are typically installed: **Type 2:** 4-strand barbed wire with 8-inch wire spacing, and **Type 6:** 32-inch woven wire with 1 strand of barbed wire on the bottom and 2 strands of barbed wire on the top. Page 9 of the "Better Roads Brochure" contains a more extensive discussion of your permanent fencing options. This brochure will be available at the meeting.

4. Temporary fencing adjacent to the highway: Do you anticipate having livestock in pastures located adjacent to the proposed project during highway construction activities?
5. Are you aware of any waterlines, drainfields, septic tanks, underground storage tanks, underground power lines, etc. that are located adjacent to the project and may be impacted by construction activities?
6. Are there any highway-related drainage or flooding problems located along your property or elsewhere along this section of highway?
7. Possible sites for gravel and additional dirt: Are you aware of potential material available for construction that might be located adjacent to the highway?
8. Temporary access during construction activities.

Please review your property and be prepared to discuss the above issues, as well as any other issues that you feel are unique to your property. No offers to acquire property will be made at these meetings since revisions to the plans will likely occur from your input.

ACCESS MANAGEMENT

South Dakota's Commitment to Safety and Smart Investment Decisions In Transportation

What is Access Management?

Access Management is the process of providing highway entrances only at locations where they can be provided safely and efficiently.

Consider that each access point added to an undivided highway in an urban or suburban area increases the annual accident rate by 11 to 18 percent on that highway segment. In rural areas, each added access point increases the annual accident rate by 7 percent. Overall, driveway-access accidents alone cost South Dakota approximately \$36.5 million each year.

Each additional access point also contributes to congestion. The more driveways on a street, the more places where people are slowing, changing lanes and turning. A seven-lane street can quickly become a parking lot when there are many driveways in each block. When that happens, our valuable transportation investments are wasted and access to adjacent businesses is restricted.

Good access depends on the following:

- Limiting the number of conflict points (places where there is a potential for crashes)
- Separating conflict areas
- Reducing interference with through traffic
- Providing good on-site circulation and storage
- Properly spaced traffic signals

How does Access Management affect businesses?

Studies have shown that access management can provide three benefits to businesses adjacent to highways:

- Making sure that drivers can get in and out of businesses without being blocked by other traffic
- Making the highway more attractive by reducing congestion
- Extending the business' effective service area by reducing travel times

These benefits come not from having many driveways, but by having well-planned, well-located, high-capacity access points on the highway.

Business owners adjacent to access management projects have expressed concern that the projects will adversely affect their operations. Most have found, however, that the projects have been beneficial.

For more information on Access Management, contact:

Rick Laughlin, SDDOT Access Management Specialist
5316 W. 60th St.
Sioux Falls, SD 57107
Phone: 605-367-4970 E-Mail: Rick.Laughlin@state.sd.us

M.G. Oil Company

1002 Main

P.O. Box 1006

Rapid City, South Dakota 57709-1006

MARLYN ERICKSON
President
TROY ERICKSON
Vice President

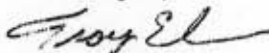


Office Phone (605) 342-1953
Office Phone (605) 342-0814
Office Phone (605) 342-0527
WATS 1-800-333-5173
FAX 1-605-341-1899

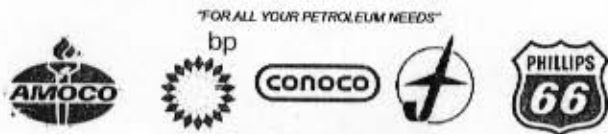
Dan Ferber,

After looking at the three alternatives involved with the exit 61 construction I believe that the Tight Diamond Interchange is the best option. It will accomplish the long term goal for this exchange with the least effect on the neighboring properties. The option that will have the most impact to my Flying J property is the Partial Cloverleaf Interchange. This option if decided upon will have a devastating impact on my facility. I don't see any way that I can operate my Truck Stop or accomplish a traffic flow pattern for Gasoline and Diesel Fuel customers with the access that is proposed for my facility. I believe that the Partial Cloverleaf Interchange if chosen will be so inconvenient for my customers that it will force me out of business. I am willing to come to any meeting at any time anyplace to help in this decision process. Please keep me informed so that I can be at them.

Thank you,



Troy Erickson
M.G. Oil Company



M. G. Oil Company

Amoco Food Shops, Flying J Truck Stops, Video Lottery
1002 Main - Box 1006
Rapid City, South Dakota 57709

TROY M. ERICKSON
Vice President

Wats: 1-800-333-5173
Fax: (605) 341-1899
Office Phone: (605) 342-0527
Mobile Phone: (605) 390-2882
email: troy@mgoil.com

Response to comments from Open House April 22, 2003

Mr. Erickson writes that after reviewing the three alternatives presented for the reconstruction of Exit 61 he prefers the Tight Diamond Interchange. He further states that the Partial Cloverleaf is his least favorite. He feels that the Partial Cloverleaf Interchange will have a devastating impact on his facility.

Response: The comments he presented relate to the joint open house held for the Eglin Street Corridor Study and the Exit 60 and 61 Interchange reconstruction. His comments have been forwarded to the SDDOT.

EGLIN STREET CORRIDOR STUDY

OPEN HOUSE

April 22, 2003

Name(s):

Address: _____

Phone Number: _____

I (We) own Property within the Eglin Street Corridor Study Area Yes _____ No _____

I (We) have the following comments regarding the revised Alignment Alternatives: _____

I (We) believe that the proposed changes will have the following impact on our property: _____

Please include any additional comments you may have concerning this project. _____

Please submit comments by May 2, 2003 to the following:

Patsy Horton
Rapid City Planning Dept.
300 Sixth Street
Rapid City, SD 57701
(605) 394-4120

OPEN HOUSE – JUNE 9, 2003

EGLIN STREET CORRIDOR STUDY

OPEN HOUSE

June 9, 2003

BACKGROUND

The City of Rapid City in conjunction with the Rapid City Area Metropolitan Planning Organization (MPO) and the South Dakota Department of Transportation (SDDOT) have contracted with Ferber Engineering Company and Interstate Engineering, Inc. to develop a Corridor Study for the Eglin Street Corridor from Lacrosse Street on the west to one-half mile east of Elk Vale Road.

Eglin Street currently exists as a two-lane road between East Anamosa Street and Elk Vale Road, providing primary access to Menards, the South Dakota Department of Transportation offices, the Kanab Pipeline Rapid City Terminal, and numerous industrial and commercial uses located south of I-90 between East Anamosa Street and Elk Vale Road.

The South Dakota Department of Transportation is planning the reconstruction of Exit 60 (East North Street), and Exit 61 (Elk Vale Road) in the next two to three years. Reconstruction of these interchanges will result in changes to the alignment of Eglin Street and reconfiguration of the intersection of Eglin Street and Elk Vale Road. The interchange reconstruction as well as the pending extension of Mall Drive from LaCrosse Street to Elk Vale Road will alter transportation infrastructure throughout the Study Area.

On February 18, 2003, an Open House was conducted to encourage property owners to comment on the information available at the Open House. A second Open House was held on April 22, 2003 which incorporated comments received from the February 18, 2003 Open House into the revised alignments. The second Open House was held in conjunction with the South Dakota Department of Transportation to receive public comment on potential interchange alignments at Exits 60 and 61.

The Open House scheduled for June 9, 2003, will be to present alignments of Eglin Street based on previous public comment received to date as well as South Dakota Department of Transportation's information on access separation requirements for Eglin Street.

STUDY OBJECTIVES

- Identify recommendations for alternate alignments for Eglin Street
- Investigate the potential impact of these proposed changes on the level of service of existing and proposed streets and intersections
- Provide recommendations to address weaving movements related to Exit 60 and 61
- Provide recommendations regarding the traffic signal installations including phasing, signal timing, and timing coordination with other existing signals
- Provide recommendations for roadway alternative impacts on the sanitary sewer collection system
- Provide recommendations regarding Dyess Avenue and its relationship to Eglin Street
- Provide recommendations regarding the continuation of Eglin Street east of Elk Vale Road

EGLIN STREET CORRIDOR STUDY

OPEN HOUSE

June 9, 2003

The comments and concerns of area landowners and residents are a significant part of our planning effort. We ask you to carefully review the revised materials presented here and share with us your comments or concerns regarding the acceptability of the various revised roadway alternatives, preference of one alternative over others, and the potential impact of the proposed corridor improvements on your property within the Study Area. A comment sheet has been attached to this packet for your use. Please return your comments by June 23, 2003 to Patsy Horton at the Rapid City Planning Department, 300 Sixth Street, Rapid City, SD 57701.

EGLIN STREET CORRIDOR STUDY

Alignment Summary

The recommended alignment for Eglin Street will include construction of two new intersections: Eglin Street and North Street, and Eglin Street and LaCrosse Street. The recommended alignment will also include reconstruction of the existing intersection between Eglin Street and Elk Vale Road. The following is a brief description of the proposed improvements at these intersections:

Eglin Street / LaCrosse Street The recommended extension of Eglin Street from North Street to LaCrosse Street will result in a new intersection being created near the present intersection of LaCrosse Street and Rapp Street. The signalized intersection will align with the existing motel approach on the west side of LaCrosse. In order to provide adequate stacking for Eglin Street, some realignment will be required to Rapp Street and the access to the Comfort Inn Motel. (see attached photo)

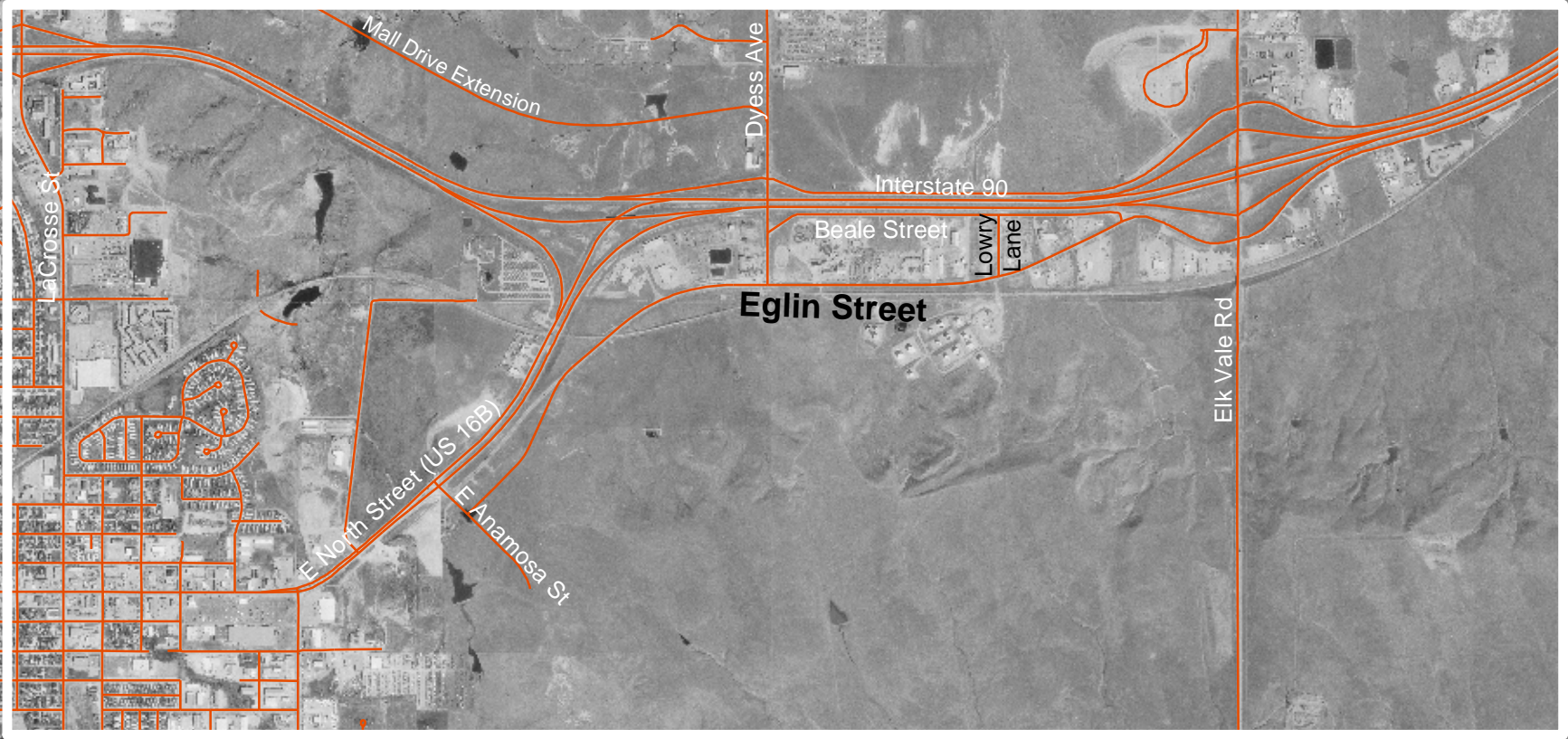
Eglin Street / North Street The recommended extension of Eglin Street will also result in a new intersection being created between Eglin Street and North Street. This signalized intersection will occur between the present North Street / DM&E Railroad Crossing and the Eastbound Ramps for the reconstructed Exit 60. The construction of this new intersection and the proposed extension of Eglin Street to LaCrosse Street will eliminate the existing Eglin Street / DM&E Railroad Crossing. The existing access will remain but will terminate in cul-de-sacs on each side of the crossing. (see attached photo)

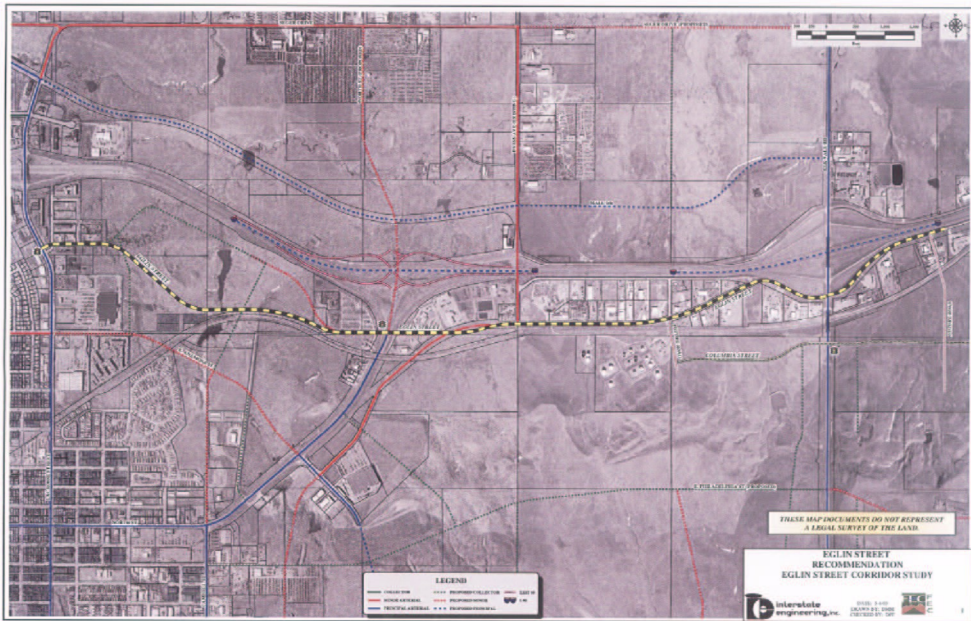
Eglin Street / Elk Vale Road Elk Vale Road is being reconstructed as an Urban Expressway. The proposed reconstruction will include reconstruction of the Exit 61 interchange. The final design of the interchange has not been decided. However, in order to protect the traffic flow on Elk Vale Road, the SDDOT has established access criteria and has purchased control of access from adjacent landowners. The spacing of the Eglin Street / Elk Vale Road intersection will require the intersection to be restricted to right-in / right-out turning movements only. In order to provide additional movement for Eglin Street traffic, a connection from Eglin Street across the DM&E Railroad will be made to the full-movement signalized intersection of Elk Vale Road and Columbia Boulevard. West of Elk Vale Road this connection will be made in the vicinity of Lowry Lane. East of Elk Vale road, the connection will be made in the vicinity of DeGeest Drive. (see attached photo)

The timing of the proposed improvements at Eglin / North will coincide with the reconstruction of Exit 60. The proposed improvements to Eglin / Elk Vale Road will coincide with the reconstruction of Exit 61. The proposed improvements to Eglin / LaCrosse will coincide with the extension of Eglin Street from North to LaCrosse Street.

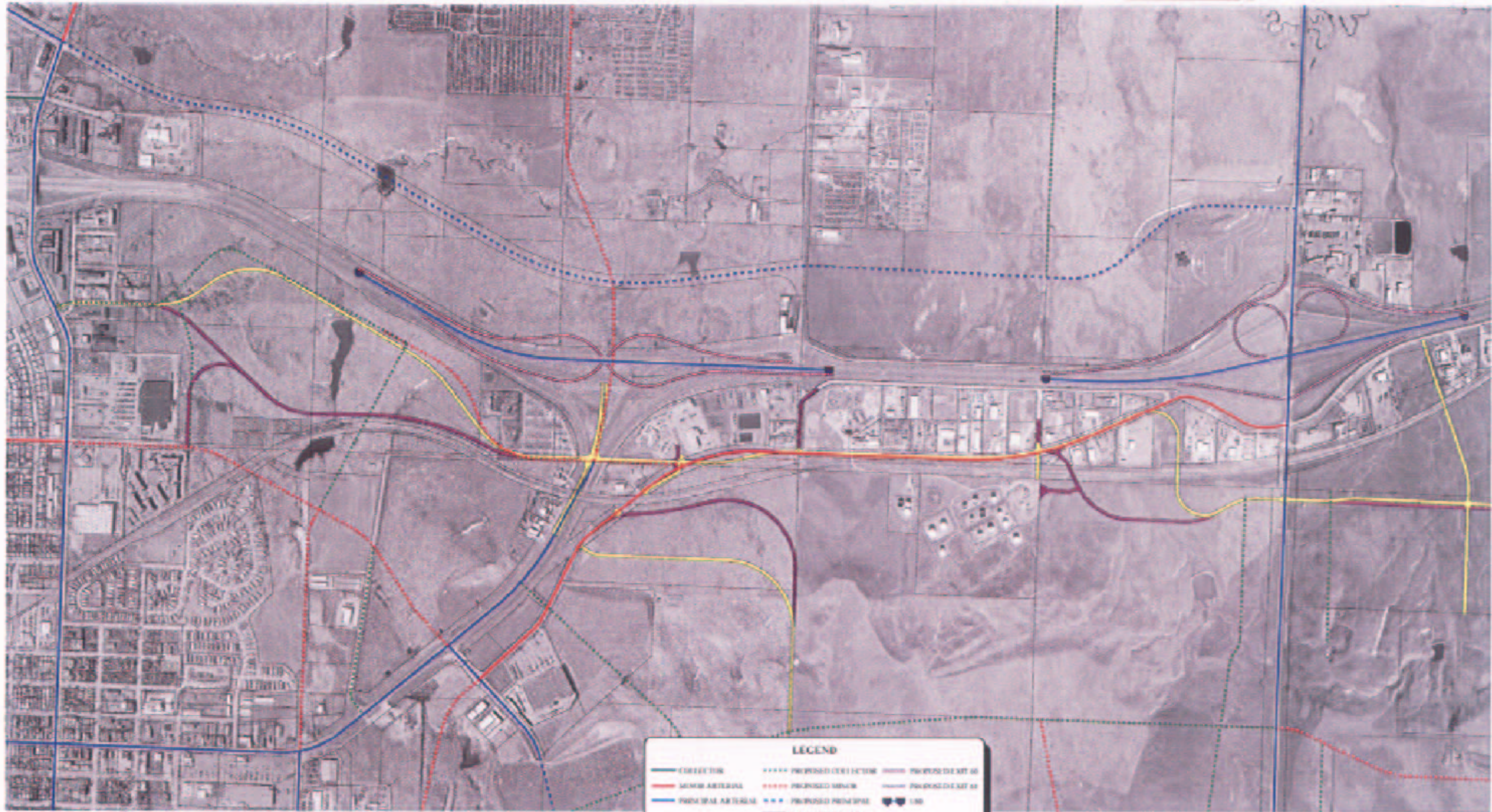
Eglin Street Corridor Study

Study Area Boundary





EGLIN STREET CORRIDOR STUDY
ALTERNATIVES #1 AND #3 ALIGNMENT

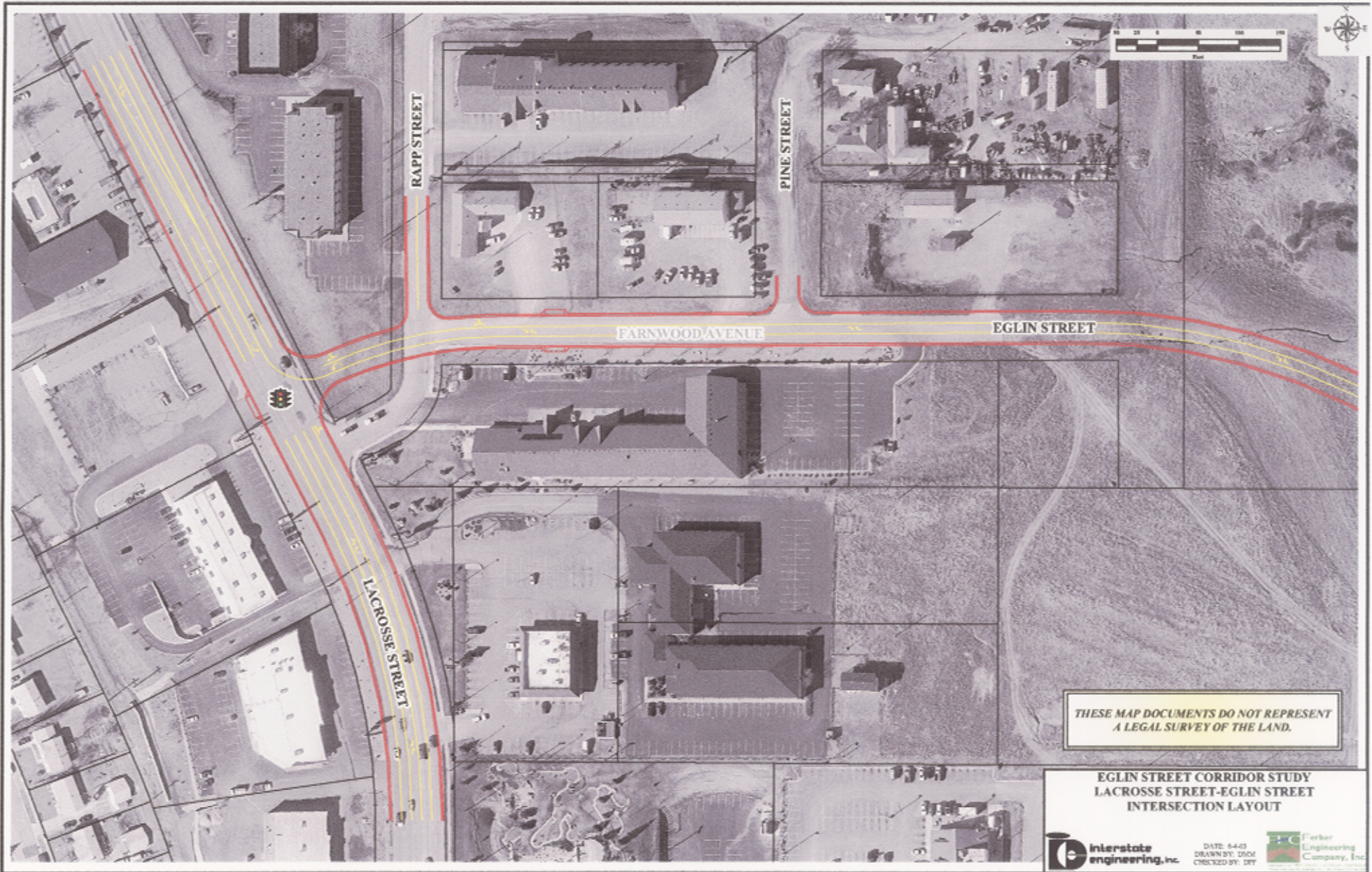


LEGEND

— COLLECTOR	--- PROPOSED UTILITY	--- PROPOSED SET 40
— MINOR ARTERIAL	--- PROPOSED SIDEWALK	--- PROPOSED SET 40
— PRINCIPAL ARTERIAL	--- PROPOSED PRINCIPAL	--- UIC
— EGLIN STREET ALT 1		
— EGLIN STREET ALT 2		

DATE: 11/15/2011
PROJECT: EGLIN STREET CORRIDOR STUDY
DRAWN BY: [Name]
CHECKED BY: [Name]

interstate engineering
11111 11th Street, Suite 100
Tomball, TX 77375
Tel: 281-351-1111
Fax: 281-351-1112
www.interstate-engineering.com



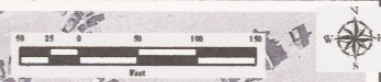
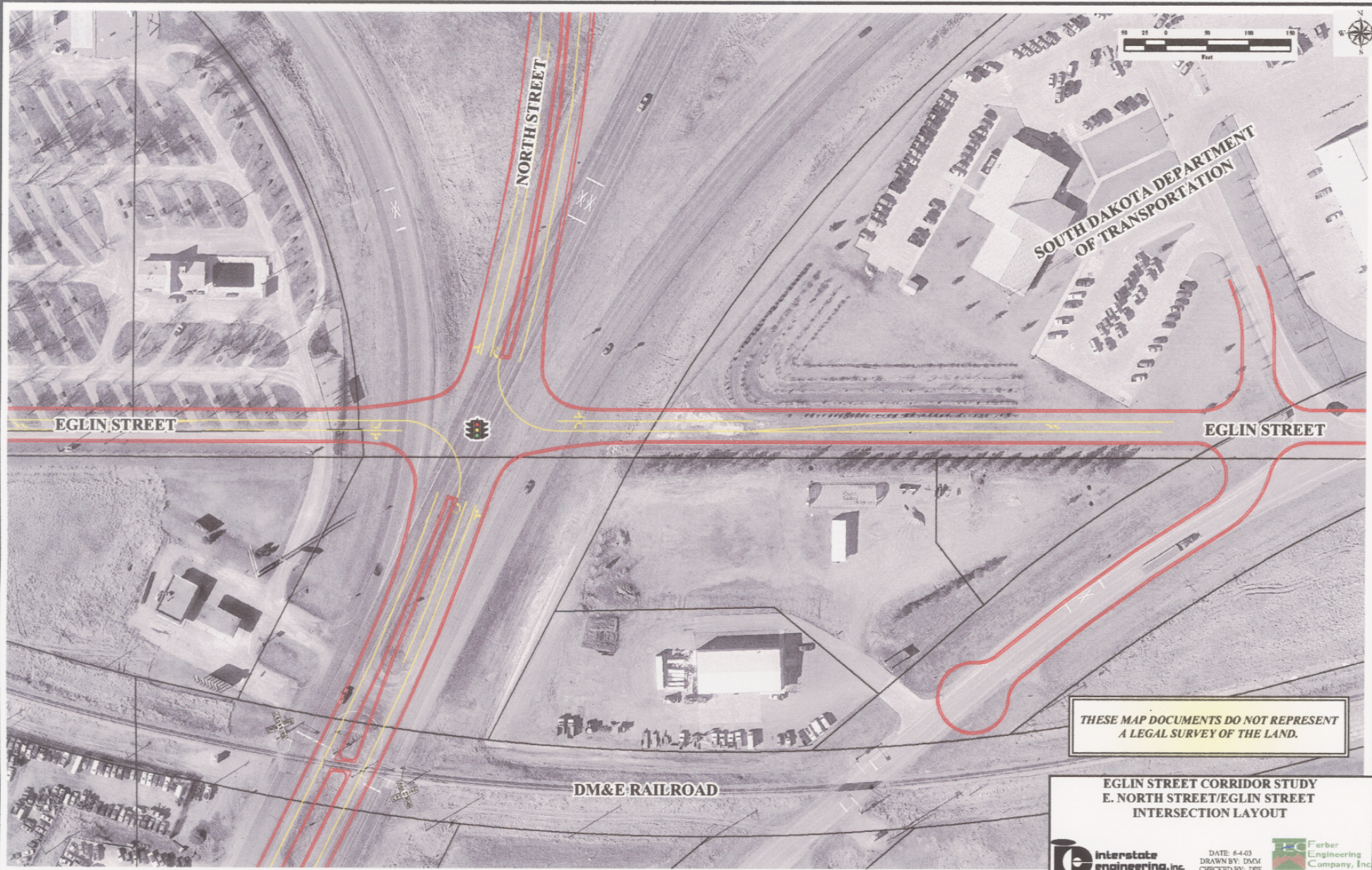
THESE MAP DOCUMENTS DO NOT REPRESENT
A LEGAL SURVEY OF THE LAND.

**EGLIN STREET CORRIDOR STUDY
LACROSSE STREET-EGLIN STREET
INTERSECTION LAYOUT**

**Interstate
engineering, inc.**

DATE: 6-4-03
DRAWN BY: DDM
CHECKED BY: DDT

FC Ferber
Engineering
Company, Inc.



EGLIN STREET

NORTH STREET

SOUTH DAKOTA DEPARTMENT OF TRANSPORTATION

EGLIN STREET

DM&E RAILROAD

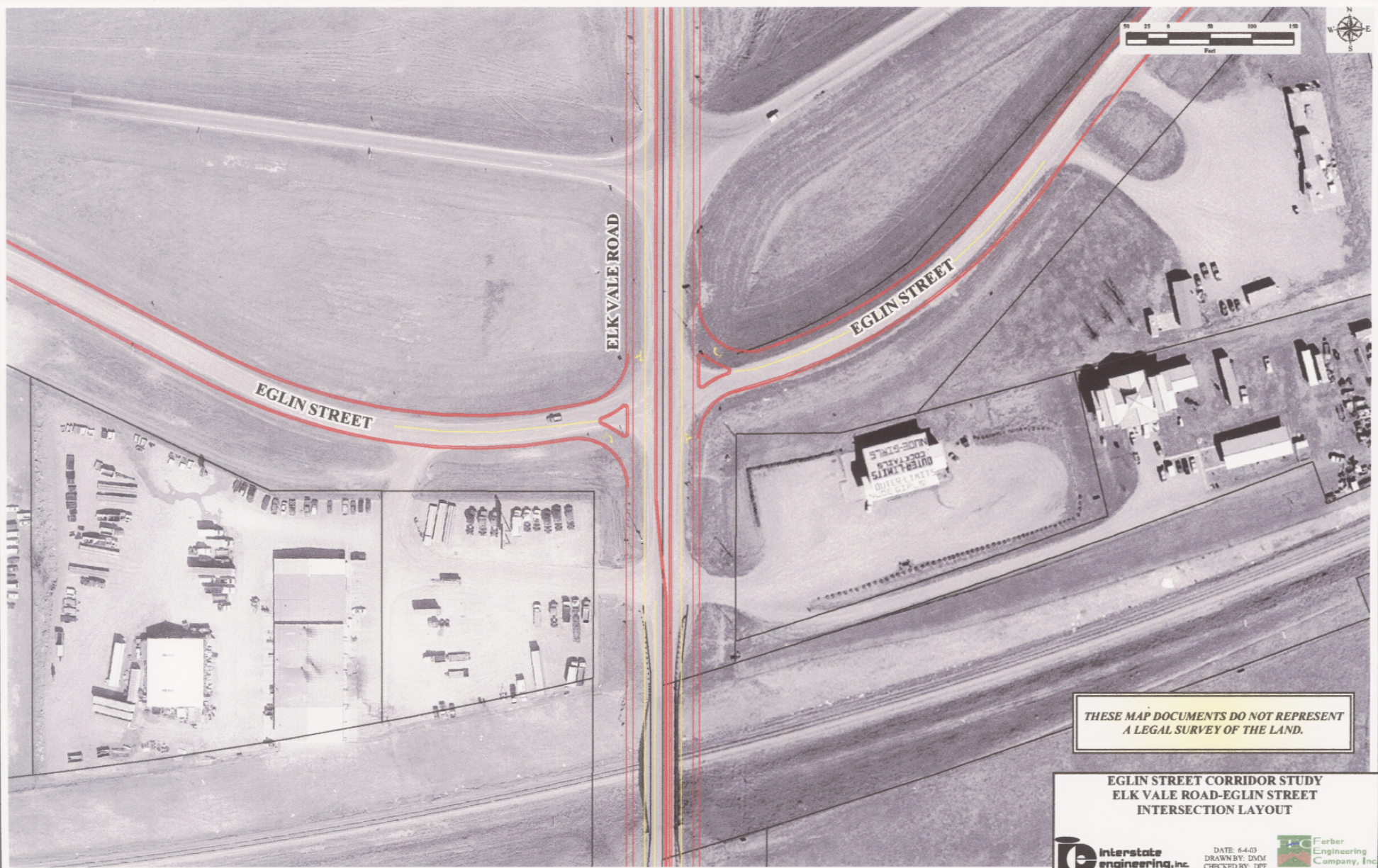
THESE MAP DOCUMENTS DO NOT REPRESENT A LEGAL SURVEY OF THE LAND.

EGLIN STREET CORRIDOR STUDY
E. NORTH STREET/EGLIN STREET
INTERSECTION LAYOUT

interstate
engineering, inc.

DATE: 6-4-03
DRAWN BY: DMV
CHECKED BY: DFF

Ferber
Engineering
Company, Inc.



THESE MAP DOCUMENTS DO NOT REPRESENT
A LEGAL SURVEY OF THE LAND.

**EGLIN STREET CORRIDOR STUDY
ELK VALE ROAD-EGLIN STREET
INTERSECTION LAYOUT**

Interstate
engineering, inc.

DATE: 6-4-03
DRAWN BY: DVM
CHECKED BY: DVF

FC Ferber
Engineering
Company, Inc.

June 23, 2003

VIA FACSIMILE AND U.S. MAIL
605.394.6636

Patsy Horton
Rapid City Planning Department
300 Sixth Street
Rapid City, SD 57701
Phone: 605.394.4120

RECEIVED

JUN 26 2003

Rapid City
Planning Department

RE: Eglin Street Corridor Study

Dear Ms. Horton:

This letter is in response to the proposed Eglin Street modification in Rapid City, SD. Menard, Inc. is the owner of the property located at 1805 Eglin Street. As part of Menard's initial approval process, we were required to improve and relocate the frontage road in front of our property. There were several benefits associated with the improvements, they are as follows: The traffic burden in at the intersection of US 16B and E Anamosa St. is lessened. The improvements we made provide an alternative route to the east with significantly less traffic as compared to US 16B. As the area matures, the land surrounding the Menard site will become more attractive to retail development. If the through access provided by Eglin is removed, a great deal of this attractiveness is gone.

In conversations with the Planning Department, it has been suggested to Menard that the reason for closing off the Eglin as it currently stands is because of a rail crossing issue. The current rail crossing at Eglin is so close to rail crossing at US 16B that one could argue it is in fact one large crossing. After inspecting the most recent proposal, there are four additional proposed crossings over the rail line. Closing off the current Eglin rail crossing seems unnecessary and inconsistent with the demands placed on Menard, Inc. just a few years ago.

The current proposal shows a proposed road, shown in green, that road will transverse the Menard property from the proposed Philadelphia St. to US 16B. That proposed road would constitute a taking and leave Menard with two sites that are not very attractive for retail development. The smaller site would be virtually worthless, while the other more easterly site would not be able to fit an additional large retailer. In Menards, there is already an anchor store in place, if access and demand are appropriate other large retailers will follow. Taking away access will ensure that neither demand nor access will ever be present at this site.



Menard, Inc. supports the majority of the plan. It is apparent that many of the changes being made will benefit the City along with our store. However, the cul-de-sacs on each side of Eglin simply do not make sense. Removing rail crossings only sets a precedence for the future, as there is little doubt that as the City grows the demand for streets and rail crossings will only increase. Eglin, as it currently exists, is not a road to nowhere, nor is under utilized. Menard, Inc. urges the City to reconsider its plan to eliminate the existing Eglin Street / DM&E Railroad crossing. If this crossing is eliminated it will only hurt Menards and future development immediately around Menards.

Should you have any questions or comments, please feel free to contact me at the numbers listed below.

Sincerely,



Greg Kunstman
Corporate Counsel
Menard, Inc.
Properties Division
Phone: 715.876.2120
Fax: 715.876.5960

EGLIN STREET CORRIDOR STUDY

OPEN HOUSE

June 9, 2003

Name(s): Sylvia Conrad
Address: 303 E. JACKSON ST. #11 - Rapid City, SD
Phone Number: 605-348-2070

I (We) own Property within the Eglin Street Corridor Study Area Yes No

I (We) have the following comments regarding the revised Alignment Alternatives: _____

No objection to the new alignment. My concern is abandoning the current Eglin access, & the Cul-de-sac at the Rail Road tracts.

I (We) believe that the proposed changes will have the following impact on our property: _____

Negative. Every access is important - & to eliminate this one does not make sense to me. This comment is only on the proposed Cul-de-sac.

Please include any additional comments you may have concerning this project. _____

I'm sorry I missed your deadline. I wanted to get with Don Weisler, but his mom passed away last week & this got over-looked.

Please submit comments by June 23, 2003 to the following:

Patsy Horton
Rapid City Planning Dept.
300 Sixth Street
Rapid City, SD 57701
(605) 394-4120

Sylvia

EGLIN STREET CORRIDOR STUDY

OPEN HOUSE

June 9, 2003

Name(s): Wood Stock Supply Inc
Address: Andrew Vignone 4705 S. I-90 Service Rd
Phone Number: 605-341-6900

I (We) own Property within the Eglin Street Corridor Study Area Yes No

I (We) have the following comments regarding the revised Alignment Alternatives: _____

Why is the alignment of Eglin Street at Elk Vale Road being offset even more than it already is? To me the East side should be moved south to align with the west side which would give more storage room for cars at the stoplight on the interchange!

I (We) believe that the proposed changes will have the following impact on our property: _____

Right in and Right out only at Elk Vale on Eglin St. is not the best alternative for Wood Stock Supply and would be disastrous if an other route is not available before right in right out is implemented

Please include any additional comments you may have concerning this project. _____

Please submit comments by June 23, 2003 to the following:

Patsy Horton
Rapid City Planning Dept.
300 Sixth Street
Rapid City, SD 57701
(605) 394-4120

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Rapid City
Planning Department

Response to comments from Open House June 9, 2003

June 23, 2003 letter from Greg Kunstman, Corporate Counsel for Menards, Inc.

Mr. Kunstman is concerned that removing the connection between the existing Eglin Street fronting Menards and the future Eglin Street crossing East North Street as presented in the third Open House will have a detrimental impact on the value of the Menards property. He also makes a case that the alignment shown in Alternative #2 would divide their property in such a way as to lower it's potential value.

Response: Alternative #2 was removed from consideration at the time that traffic studies showed that the Eglin Street / East North Street intersection would not lower the overall operational characteristics, thus eliminating the concern for the Alternative Alignment.

Rapid City requested that existing Eglin Street fronting Menards remain connected to the proposed Eglin Street intersecting East North Street. The recommended alternative shows that the connection exists.

Undated comment sheet from Sylvia Conrad after the Third Open House.

Ms. Conrad writes that she has no objection to the new alignment. She is concerned with the proposed disconnect of the Eglin Street fronting Menards from the Eglin Street intersecting East North Street as shown at the Third Open House. She views that disconnect as negative.

Response: Rapid City requested that existing Eglin Street fronting Menards remain connected to the proposed Eglin Street intersecting East North Street. The recommended alternative shows that the connection exists.

Comment Sheet received from Andrew Vig on June 24, 2003 by the Rapid City Planning Department.

Mr. Vig asks "Why is the alignment of Eglin Street at Elk Vale Road being offset even more that it already is? To me the east side should be moved south to align with the west side which would give more storage room for cars at the stop light on the interchange." He believes that right-in and right-out only at Elk Vale Road on Eglin Street is not the best alternative for Woodstock Supply and would be disastrous if another route is not available before right-in/right-out is implemented.

Response: The recommended alternative for Eglin Street includes a full movement intersection with Elk Vale Road. Mr. Vig is apparently confused by the myriad of names shown on the study maps. Eglin Street is shown as becoming Cheyenne Boulevard after crossing Elk Vale Road, but the route is continuous.

The recommendations include a secondary connection between Eglin Street and the existing S I-90 Frontage Road that accesses Woodstock Supply. The timing of construction is dependent on availability of funds and right-of-way, as well as securing a crossing permit from the DM&E Railroad.