



CITY OF RAPID CITY

RAPID CITY, SOUTH DAKOTA 57701-2724

Growth Management Department

300 Sixth Street

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Community Planning Division
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MEMORANDUM

TO: City Council
Legal and Finance Committee

FROM: Karen Bulman, Planner I *KB*

DATE: August 4, 2004

RE: Gwendolyn L. Stearns Foundation grant

The Rapid City Beautification Committee is requesting permission to apply for a grant from the Gwendolyn L. Stearns Foundation in the amount of \$50,000.

The Committee is requesting the funds to be used as a funding match for a subsequent grant application for a Federal Transportation Enhancement Grant. The funds would be used to beautify the downtown areas with additional plantings of trees, shrubs, and other greenery at the "Corner Parks". If the Federal Transportation Enhancement Grant is not accepted, the Beautification Committee is requesting to use the money to beautify the downtown areas as much as possible.



EQUAL HOUSING
OPPORTUNITY

EQUAL OPPORTUNITY EMPLOYER

Gwendolyn L. Stearns Foundation

Application for Funding

Your organization has been selected by the Board of Directors of the Gwen Stearns Foundation to apply for a 2004 grant.

Please complete the following. A final decision on your request will be made in the Fall of 2004.

Date of Application: **8-2-2004** Organization Tax ID Number: **N/A**

Name of Organization: **Rapid City Beautification Committee**

Address: **300 6th Street – C/SAC Building**

City: **Rapid City** State: **South Dakota** Zip Code: **57701**

Telephone: **605 394-4120** Fax:

Primary Contact: **Jim White** Title: **Committee Chairman**

Telephone: **605 394-4033 / 341-4412** Fax: **394-1889**

Organization Information

Brief history/purpose of your organization (Mission Statement):

See attached Mission statement within the By-laws.

Geographic area served: (Projects outside of Rapid City will not be considered))

See attached "Mayor's Beautification Committee – Prioritized Project List"

If your organization operates from multiple locations, please provide the address for the location where the requested funds will be used:

N/A

Purpose of Grant request:

Explanation should reflect the grant purpose in some detail, including exactly who benefits and how the community is served.

Our request is a contingent request consisting of two parts:

Part One Contingency: Use the granted funds to apply for a Federal Enhancement Grant match on a 25%-Beautification Committee -75%-Federal Enhancement Grant match. These funds will be used to beautify the downtown areas additional planting of trees, shrubs, and other greenery at the "Corner Parks." Additionally, when Main and St. Joe Streets were reconstructed we had irrigation sleeves installed for future irrigation piping lines to be run under the sidewalks. These will insure that this committee's continued beautification projects will thrive. Our goal is to beautify the downtown areas from East Boulevard to West Boulevard with trees, shrubs, etc. Only federal funds will be used to cover the costs of any construction necessary

Part Two: In the event the Federal Enhancement Grant is not accepted, we will use the Gwendolyn Stearns funds in full to beautify the downtown area as much as possible as stated in the above paragraph.

Amount requested: **\$50,000**

(Note: Funding requests for building/construction will not be considered)

Attach the Following Documents:

- A copy of your organizations most recent tax exemption letter from the IRS.
This committee is a volunteer committee falling under the direction of the Rapid City City Council and Mayor, therefore no IRS tax exemption letter exists for this committee.
- A list of names of your Board of Directors and Officers
- A Project budget detailing project expenses
- Copy of your Articles of Incorporation and any amendments
- A copy of your latest Form 990 files with the Internal Revenue Service

BYLAWS OF THE CITY OF RAPID CITY BEAUTIFICATION COMMITTEE
October 2001

MISSION STATEMENT: *It is the mission of the City of Rapid City Beautification Committee to develop and implement beautification projects and policies, to discuss pertinent issues, and to encourage public and private beautification activities in Rapid City.*

1. The City of Rapid City Beautification Committee shall consist of twelve (12) voting members recommended for appointment by the Mayor and approved for appointment by the City Council.
2. The Beautification Committee shall have one regularly-scheduled monthly meeting. Additional meetings may be called by the Chairperson as deemed necessary. Beautification Committee meetings are open to the public.
3. Members shall be limited to a three-year term, however, the Mayor can re-appoint members to additional terms. The terms of the initial members shall be staggered in order to ensure continuity in membership.
4. A chairperson and vice-chairperson will be elected by the Committee at the December meeting every year. The chairperson and vice-chairperson shall be elected from the membership of the Beautification Committee. The chairperson and vice-chairperson shall serve a one-year term (January through December of the year following her/her election). The vice-chairperson shall conduct the meeting in the absence of the chairperson.
5. In addition to the full committee, ad/hoc subcommittees will be sued for major projects. A member of the full committee shall chair the ad/hoc subcommittee. Members of the ad/hoc subcommittee can include other full committee members or outside volunteers. Ad/hoc subcommittees will be created and dissolved as projects, issues and policies dictate. If an ad/hoc subcommittee has not met for one year, then such committee shall be deemed dissolved. The Beautification Committee shall maintain a list of all committee members and any active subcommittees and the members thereof.
6. City staff assistance well be available to the Rapid City Beautification Committee and its ad/hoc subcommittees. The City Planning Department will continue to provide the majority of this support, however, support form other City departments such as the Public Works department, Parks Division, Building Inspections Division, and Community Development Department will also be available. Staff shall serve as ex-officio non-voting members.
7. The Mayor may recommend for appointment a City Council member to serve as a liaison to the Committee. They City Council shall confirm any such appointment made by the Mayor. City Council liaisons shall not have voting authority.

8. The Chairperson shall have the discretion to recommend to the Mayor that a member be replaced after he/she has missed three consecutive regular monthly meetings.
9. The Beautification Committee shall maintain a list of priority projects and issues which shall serve as the official work program from which the Committee will operate. The projects and issues lists shall be updated by the Committee annually.
10. The Beautification Committee shall prepare an annual report for presentation to the City Council. The annual report shall provide an update of the current status, accomplishments, and future direction of the Committee.
11. In conducting their meetings, and unless otherwise noted in the Committee Bylaws, the Beautification Committee shall follow *Robert's Rules of Order*.



Rapid City Beautification Committee

Rapid City Beautification Committee
(formerly known as the Mayor’s Beautification Committee)

PRIORITIZED PROJECT LIST

1. Omaha Street widening/landscaping project – *in progress*
2. West Memorial Park Master Plan¹ / Landscape Design
3. ①St. Joseph Street Trees (Omaha Street to East Boulevard)
4. ④Mt. Rushmore Road Street Trees (Omaha Street to Kansas City Street)
5. ②St. Joseph Street Trees West Boulevard to 7th Street)
6. ③Main Street Trees (7th Street to West Boulevard)
7. I-90 East Welcome Sign
8. Omaha Street landscaping (north side of Omaha Street from East Boulevard to Omaha Street bridge)
9. East Boulevard Corridor – Section II (Philadelphia Street south to St. Joseph Street)
10. East Boulevard Corridor – Section III (St. Joseph Street south to top of Signal Hill)
11. East Boulevard Corridor – Section I (Madison Street south to Philadelphia Street)
12. West Boulevard Landscape Master Plan² (Omaha Street south to St. Joseph Street)
13. Omaha Street Landscape Master Plan³ (West Boulevard to Mtn. View)
14. Mt. Rushmore Road landscape Master Plan⁴ (Kansas City to Cathedral)
15. East North Street Master Plan⁵
16. West Chicago Landscape Master Plan⁶ (Mtn. View to South Canyon Road [city limits] – *some of this along W. Chicago has already taken place.*)
17. East Main Street Landscape Master Plan⁷ (Steele Street to Minnesota Avenue)

FOOTNOTES

Prioritized Targeted Areas within the Gwendolyn L. Stearns Funding

- ① Targeted for Gwendolyn L. Stearns Grant
- ② Targeted for Gwendolyn L. Stearns Grant
- ③ Targeted for Gwendolyn L. Stearns Grant
- ④ Targeted for Gwendolyn L. Stearns Grant – if funding exists to add this project

MASTER PLANS IN EXISTENCE WITHIN THE CITY DEPARTMENT

- ① West Memorial Park^A Master Plan – plans by the City’s Engineering Department
- ② West Boulevard Landscape Master Plan - plans by the City’s Engineering Department
- ③ Omaha Street Landscape Master Plan – plans by the City’s Engineering Department
- ④ Mt. Rushmore Road landscape Master Plan - East Main Street Landscape Master Plan
- ⑤ East North Street Master Plan - plans by the City’s Engineering Department
- ⑥ West Chicago Landscape Master Plan - plans by the City’s Engineering Department
- ⑦ East Main Street Landscape Master Plan - plans by the City’s Engineering Department

^A West Memorial Park is located on the north east corner of I-190 & West Boulevard adjacent to the Lower Turn around and CHS tennis courts

RAPID CITY BEAUTIFICATION COMMITTEE

Board of Directors

The Mayor and City Council
Growth Management Department

Committee Officers

Jim L. White, Chairperson
Craig Grotenhouse, Vice-Chairperson

Committee Members

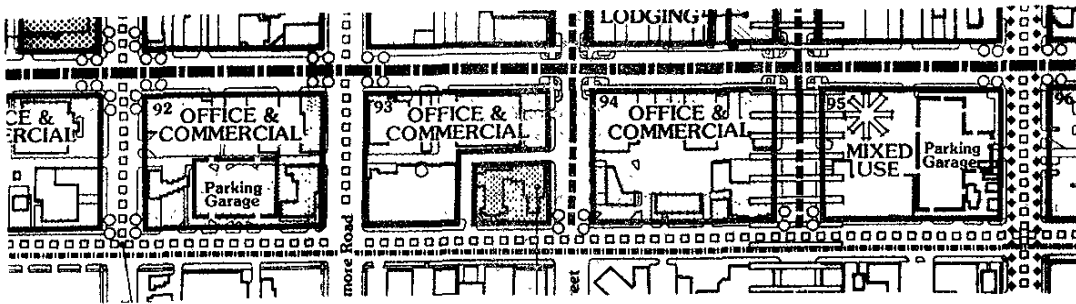
Jacque Allen, member
Martin Busch, member
Steve Doshier, member
Joel Jundt, member
Patria Pummel, member
Mike Stanley, member
Larry Stoner, member
Lou Williams, member
John Wrede, member

City of Rapid City Liaison Members

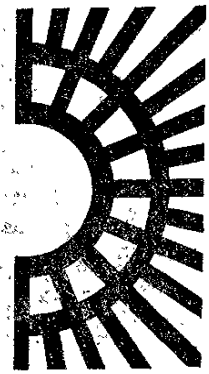
Karen Bulman, Growth Management Department
Andy Chelbek, Code Enforcement
Lon VanDeusen, Parks & Recreation
Karen Olson, City Council Representative

Tla 113

RAPID CITY



DOWNTOWN MASTER PLAN



January 9
1991

Prepared By:

Bell Galyardt Associates
DHM, Inc.
Leigh, Scott & Cleary, Inc.

8. Urban Design/Streetscapes

a. Objectives

As stated previously, one of the primary objectives of this Master Plan is to "improve the physical appearance of Downtown and create a scale on the streets which is safer and more conducive to pedestrian use". In order to carry out this objective, a multitude of design components have been studied, with recommendations regarding each area of concern offered in the following text.

b. Design Considerations

A primary consideration in the development of streetscapes in Downtown should be the creation of a comfortable outdoor space for all users. Generally, this will call for the introduction of more elements in the right-of-way, in both the vertical and horizontal planes (with "elements" referring to trees and other plant materials, lights, signage, benches, trash containers, pavements, etc.). These elements will help to meet many basic human needs by providing a sense of enclosure, adding visual interest, creating opportunities for rest and protection from the elements, and establishing a setting for social encounters. Given this increased level of comfort, people will tend to linger longer and take greater advantage of the goods and services available in Downtown.

The palette of elements used should provide both consistency and variability. Consistency is necessary to convey a wholistic treatment of Downtown, while variability is essential to maintain interest and prevent a tiresome predictability. For this Master Plan, consistency is created through the use of similar materials and colors, while variety is achieved through slight variations in style, adjustments from block to block in number and placement of elements, and use of an array of plant material species for different effects.

Specifically, recommended treatments include the following for lights, signage, and street furniture:

Materials:

Metals are proposed as the primary element. These are durable, allow for a variety of shapes and sizes, and can be designed to suggest the materials used during the historic period during which Rapid City rose to prominence.

Style:

While reference to the late 19th and early 20th centuries is desirable, exact mimicry of this period is not appropriate, given the numerous eras which are represented Downtown. Therefore, contemporary interpretations of historic styles, with a moderate level of detailing, are proposed for all streetscape elements.

Colors:

One of the most critical factors in tying together the package of furnishings is the consistent use of color. All metal components must be painted the same color, with the consistent use of a secondary accent color (sign faces, etc.) also being permissible. This will provide a subtle but necessary clue that one is in Downtown. In order to be "timeless", the selected color should be basic and subdued, with recommended colors including black, dark green, dark blue, or dark gray.

Final design of each streetscape element will be completed in a Design Development phase, outside the scope of this Master Plan. Local fabrication of elements should be pursued where feasible.

In this effort to improve Downtown, the potential exists that the "look" created could be similar to that of many other cities in the United States. Therefore, in order to create a sense of identity and distinction in the eyes of seasoned travelers (of which there should be many, given the local selection of tourist attractions), consideration should be given to emphasizing facets of local interest, such as the Native American heritage, sculptural interests (typified by Mt. Rushmore and Crazy Horse), and Black Hills historic elements. These themes could be reflected in pavement patterns, signage motifs, open space/plaza designs, and in the use of sculptures. In addition, a touch of humor is almost always appreciated by residents and visitors alike, and can be simply achieved through the use of understatement, irony, or wit in design. The distinction created by the use of any of these techniques can be a very powerful tool in gaining nationwide recognition and an increased share of the national and world marketplace.

Recommendation:

Actively pursue the development of more comfortable outdoor spaces in Downtown which reflect the local heritage.

c. Street Sections

In order to establish a framework for development of streetscapes, three representative cross-sections have been developed for Downtown streets. These are intended to represent a general pattern of development for each appropriate segment, with the expectation that detailing will vary as needed to respond to existing conditions. The sections (A, B, and C) are shown on the following pages and are also indicated on the Master Plan.

Street Section A is applicable to the full length of Main Street and to St. Joseph Street from West Boulevard to Fourth Street. Three one-way traffic lanes will remain, as will 45° diagonal parking. Sidewalks will remain at their present width of 15 feet, with improved pavement (colored concrete or modular concrete or brick pavers), added street trees and planted areas, revised signage, and limited placement of benches, trash containers, and bicycle racks.

*

Street Section B is applicable to St. Joseph Street from Fourth Street to East Boulevard. Three one-way traffic lanes will remain, as will parallel parking. Sidewalks will be widened to 23 feet, with improved pavement, significant addition of plant materials, revised signage, and limited placement of benches, trash containers, and bicycle racks.

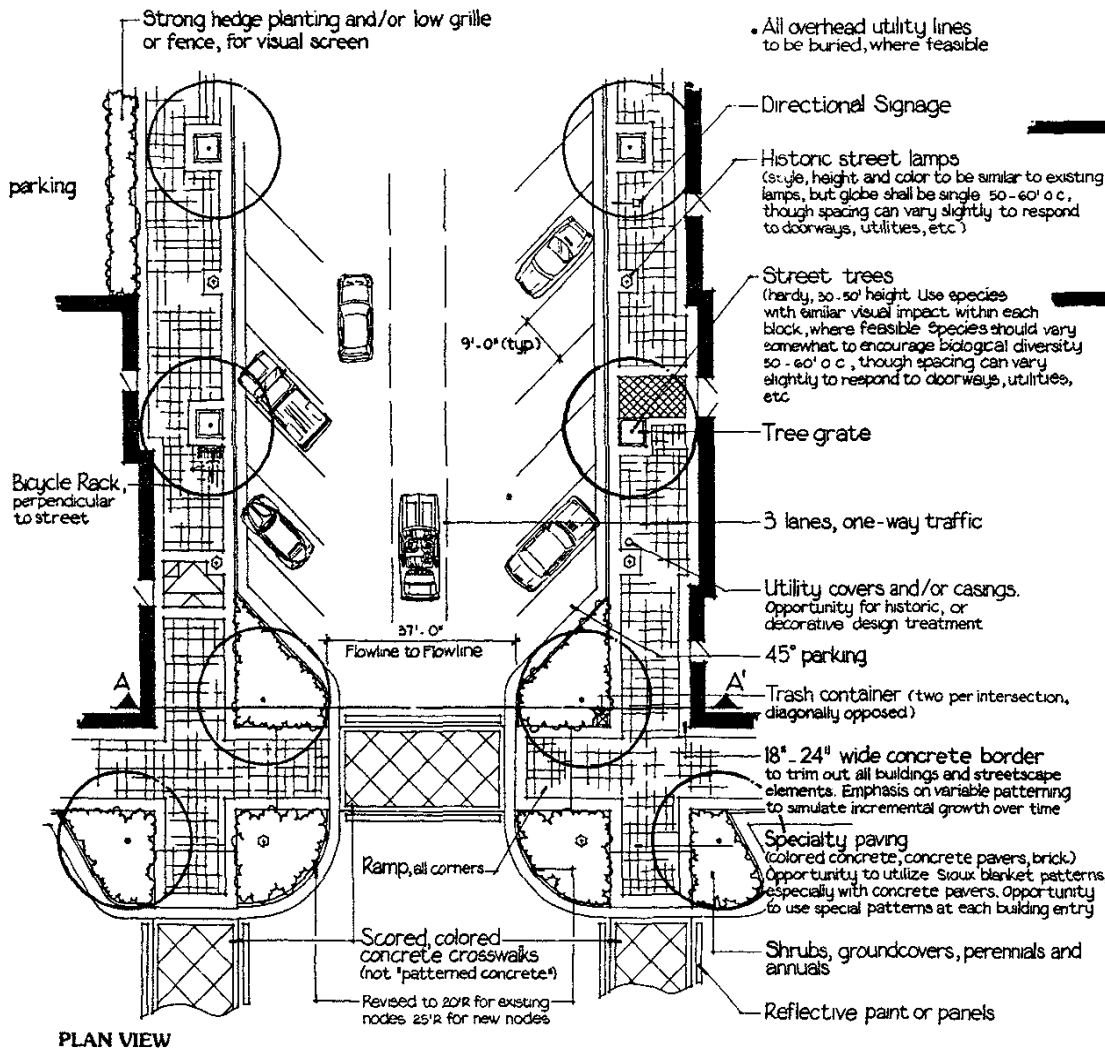
Street Section C is applicable to many of the north-south streets in the study area, including Seventh, Sixth, and Fourth through First. Many of these streets have three traffic lanes, while only two are needed for the foreseeable future. Therefore, the recommended street section has two lanes (with two-way traffic), sidewalks widened to 19 feet, and other elements similar to street Section B. Where three lanes are needed, for turning movements, the street section shall closely resemble "A".

Landscape "nodes" are shown on all three representative street sections. These nodes increase safety by shortening the distance of the unprotected pedestrian crossing. In addition, the nodes provide a means of improving the aesthetics of the right-of-way through use of tree and shrub plantings while also providing greater area for installation of street furniture such as lighting, benches, and trash containers. Specific locations where landscape nodes are recommended are shown on the Master Plan. All proposed nodes will be smaller in area than existing nodes, in order to minimize loss of parking and to offer a transition to the historic core area.

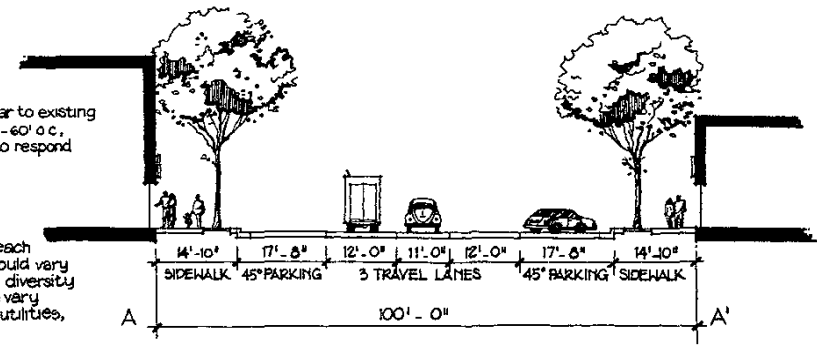
Ramps are proposed on all corners to improve access for all citizens.

In order to improve the visual quality of the entry into Downtown from the west, reconstruction of West Boulevard is recommended, returning it to its tree-lined character of the recent past. This can be accomplished in the existing right-of-way by creating a planted median 12-14 feet in width and planting strips 10-12 feet in width on both sides. The biggest obstacle to this proposal is the inefficient parking along West Boulevard for Block 81. In the long term, this parking would be accommodated in the redevelopment projected for this Block. In the short term, consolidation of these spaces with those in the lots to the east, through reconfiguration, appears to be feasible. (See Diagram on Street Section "A", on the following page.)

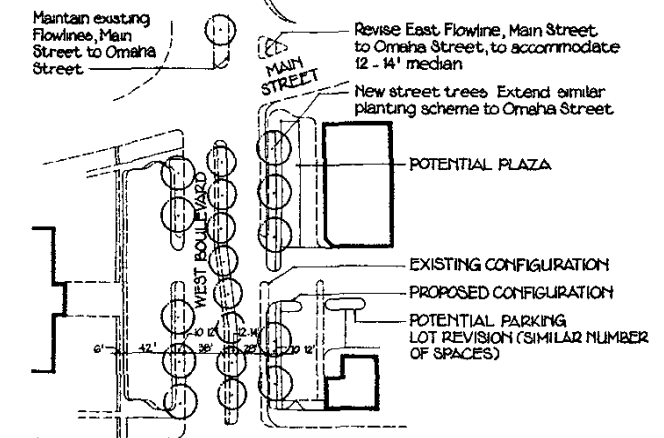
It is anticipated that many of the improvements proposed for Main Street will be installed (or allowance made for future installation) with the pending reconstruction from East Boulevard to West Boulevard. Should the reconstruction of St. Joseph Street also proceed as scheduled 1-2 years after Main Street, proposed streetscape improvements would also be installed concurrently with reconstruction, as anticipated with Main Street. Other streetscape projects and street reconstructions proposed as a part of this Master Plan would be undertaken



• All overhead utility lines to be buried, where feasible



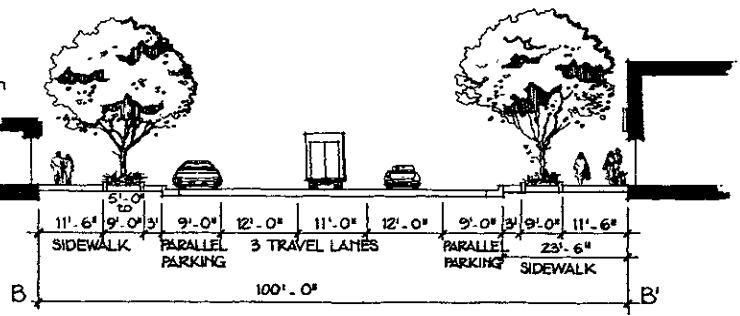
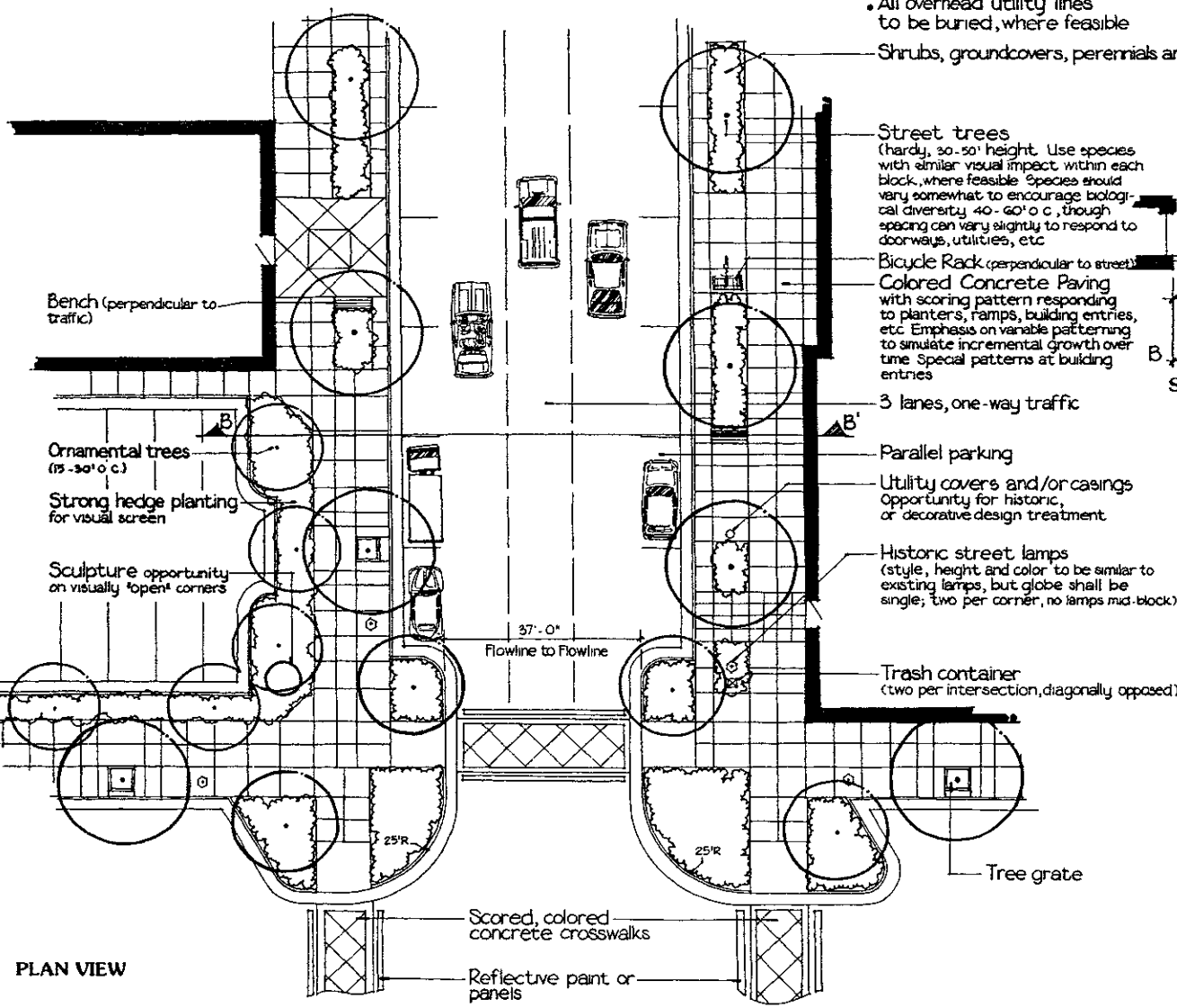
SECTION
Main Street, from West Boulevard to East Boulevard
St. Joseph Street, from West Boulevard to Fourth Street



WEST BOULEVARD REPRESENTATIVE RENOVATION
NOT TO SCALE

TYPICAL STREET SECTION "A"
DOWNTOWN MASTER PLAN
RAPID CITY, SOUTH DAKOTA

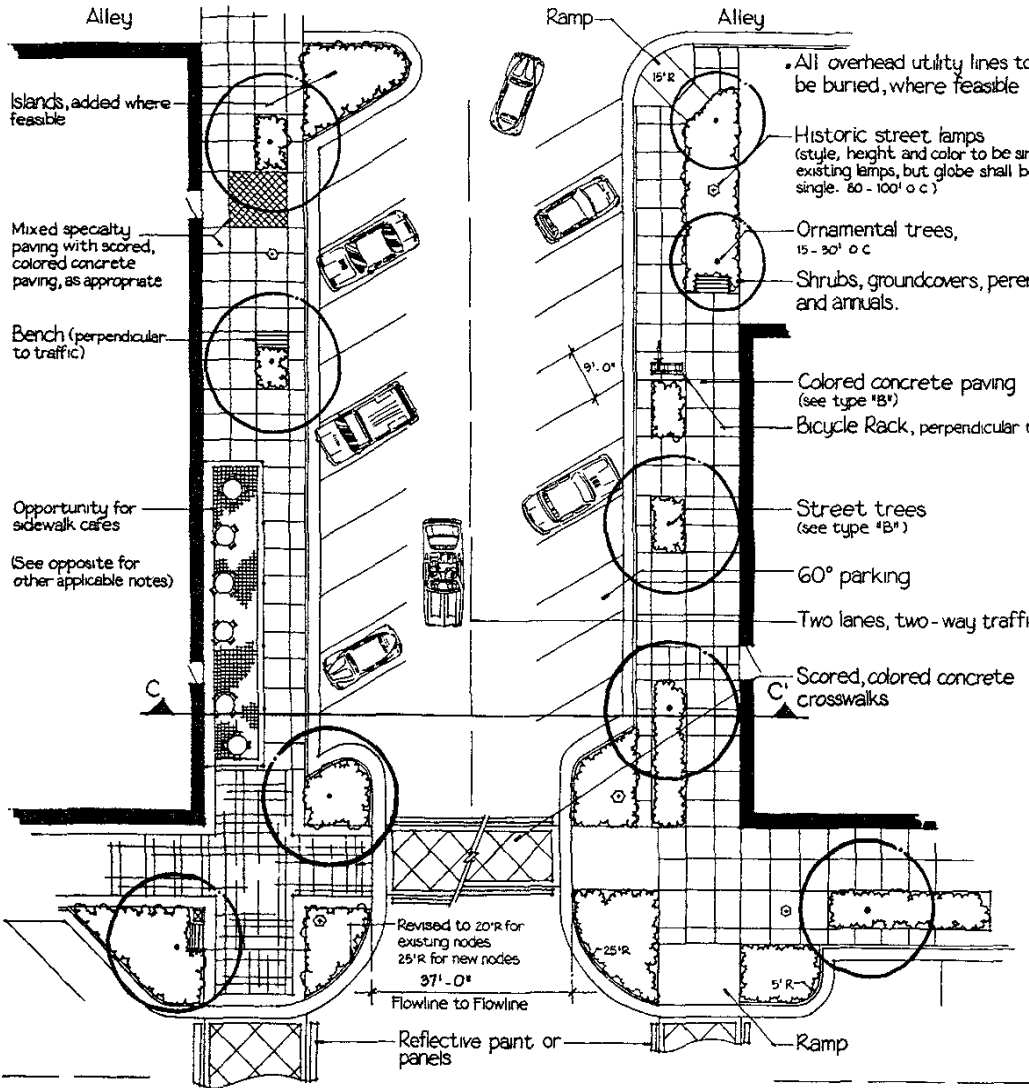
JANUARY 8, 1991
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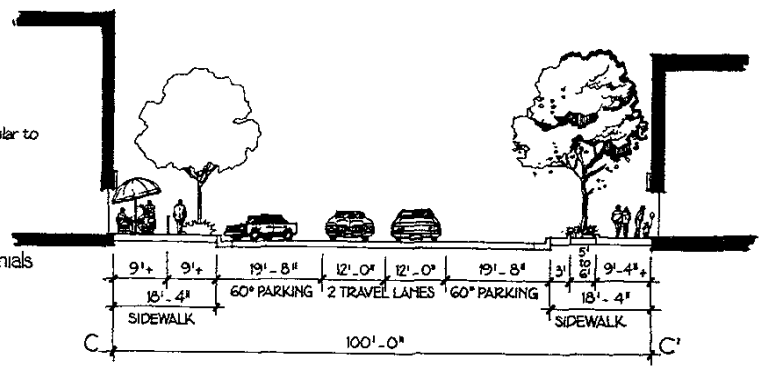
TYPICAL STREET SECTION "B"
DOWNTOWN
MASTER PLAN

JANUARY 8 1991
 SCALE: 1"=10'-0"

RAPID CITY, SOUTH DAKOTA

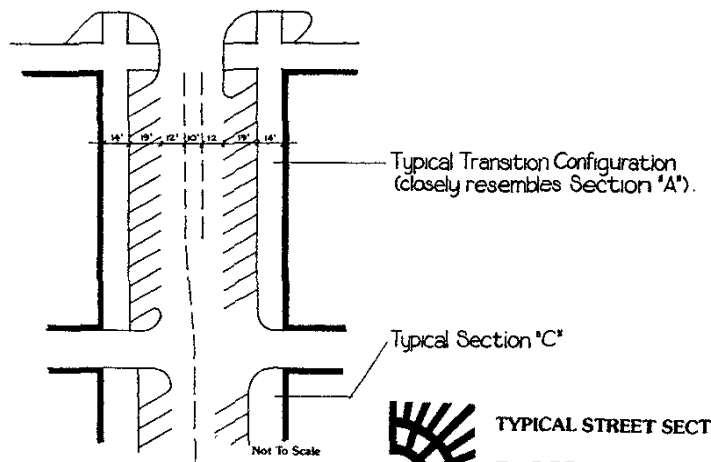


- All overhead utility lines to be buried, where feasible
- Historic street lamps (style, height and color to be similar to existing lamps, but globe shall be single. 60 - 100' o c)
- Ornamental trees, 15 - 30' o c
- Shrubs, groundcovers, perennials and annuals.
- Colored concrete paving (see type "B")
- Bicycle Rack, perpendicular to street
- Street trees (see type "B")
- 60° parking
- Two lanes, two-way traffic
- Scored, colored concrete crosswalks



SECTION
First, Second, Third, Fourth, Sixth, and Seventh Streets
from Omaha Street to Kansas City Street

NOTE: Where turn lanes are needed (see Master Plan), transition from Section C to 3-lane configuration shall be accomplished as shown below



PLAN VIEW
(Left side representative of 6th and 7th Streets, right side representative of 4th, 3rd, 2nd and 1st Streets)

either at the initiative of the City, at the initiative of owners adjoining affected streets, or concurrent with any reconstruction of properties adjoining affected streets.

The means by which any private sector improvements would be encouraged should be addressed in the formulation of the proposed overlay district or rezoning for Downtown.

Recommendation:

Install the recommended street sections as opportunity permits.

d. Alleys

Alleys in the Downtown area provide an essential means of access to service a number of existing buildings. However, many of these alleys are an eyesore, due to the clutter of the overhead power lines, the collection of trash dumpsters, and the deteriorating quality of the pavements.

In addition, the narrowed alleys restrict their use by the service vehicles for which they were intended. While a 20' right-of-way is available in the alleys, the power poles and dumpsters often reduce the usable width to 12 feet. Increasing the usable width to at least 15-16 feet would allow many more service vehicles to use the alleys than can at present.

Several potential remedies are available. First, burying of the power lines will eliminate much of the visual blight, while removal of the support poles will increase the available width for service trucks. Black Hills Power and Light has indicated that placement of lines underground is feasible, provided that adjoining property owners are willing to accommodate the change by relocating some meters and providing room for at-grade transformers. Initial estimates by Black Hills Power and Light indicate that the cost for conversion from overhead to underground wiring will be at least \$250,000.00 per block. There will also be additional costs for repaving alleys and for upgrading, relocating, and rewiring individual building electrical systems to connect to the new underground system. Further investigation is required to determine exactly where such underground placement can occur.

Second, trash containers should be moved out of the minimum 12-15' wide travel lane and placed in niches between buildings, where feasible. In addition, screening of the containers from views at the ends of the alley should be installed where possible.

Finally, concurrent with burying of lines, new paving should be installed in all alleys which have a deteriorated surface at present.

For all new construction adjoining existing alleys, allowance should be made within the footprint of the building for required at-grade transformers and for screened trash container locations. For larger projects, elimination of the alley could be considered, provided that utility and service needs are met in an aesthetically pleasing manner.

Continued use of the alleys for service needs of all existing buildings is strongly encouraged, in order to keep primary streets clear for commerce and visitors.

Recommendation:

Place utilities underground, screen trash enclosures and repave alleys, in order to improve the function and visual quality of the alleys.

e. Open Space/Plazas

Usable public open space in the Study Area is currently very limited, with the one noteworthy tract being the lawn west of the County Courthouse. Other small improvements occur at the four existing streetscape nodes and with several private office entry plazas. This general lack of open space results in a lack of gathering places, lunch places, and areas which offer relief from the hard surfaces of buildings and pavement. This sets Downtown in marked contrast to most of its surroundings and does not encourage use of the streets by shoppers, office workers or tourists.

No Downtown sites suitable for development of a typical "park" appear to be available at this time. Therefore, creation of open space appears to be most feasible in the form of urban plazas, developed in conjunction with larger scale mixed-use projects. Three potential sites have been identified on the Master Plan, with a typical program calling for a corner plaza, ground-level retail uses bracketing the plaza, office uses, and structured parking. The sketch on the following page illustrates important principles which should be followed in order to create an exciting, functional space which can reflect selected aspects of South Dakota's natural and cultural heritage. The actual design may differ substantially from that shown, of course.

Recommendation:

Integrate open space into Downtown through development of urban plazas, created via public/private joint ventures in conjunction with needed mixed-use projects.

f. Pedestrian Spine

Throughout this Master Plan, improvements for pedestrian use are encouraged. One of the primary streets which would benefit from a high level of pedestrian activity is Sixth Street. This street appears to offer the most enjoyable means of moving north-south in the Downtown area, while also providing connections to three prominent sites outside the Study Area-the Civic Center/Memorial Park Complex, Central High School and Dakota Junior High School. The existing setting on Sixth Street supports a modest level of pedestrian activity, with the City/School building, the Transportation Center, retail, restaurant, lodging, and office uses all fronting on Sixth Street. Further pedestrian activity would be encouraged by developing a more continuous facade of ground level retail uses, indoor and outdoor restaurants, entertainment facilities, open space plazas, public restrooms, and an improved streetscape (Street Section C"). This package of improvements should greatly increase

Numerous sub-spaces within the plaza, for comfortable use with smaller numbers of people; concurrently, entire plaza can be "read" as one space for large events, by keeping mid-story visually open (which also aids visibility to retail shops)

Great variety in forms, patterns, and materials to maintain user interest

Typical sculptures

Space for potential exhibits, sculpture, etc

Benches

SIDEWALK CAFE

RESTAURANT

Historic street lamps

Planters, with a variety of tree species, low shrubs and groundcovers (no mid-story, in order to keep views across plaza open)

Interwoven, multi-colored paving patterns (feasible with concrete pavers, specialty brick pavers, or custom concrete pours), representative of the multitude of cultures and their associated styles in Rapid City

Paving patterns emphasize entries

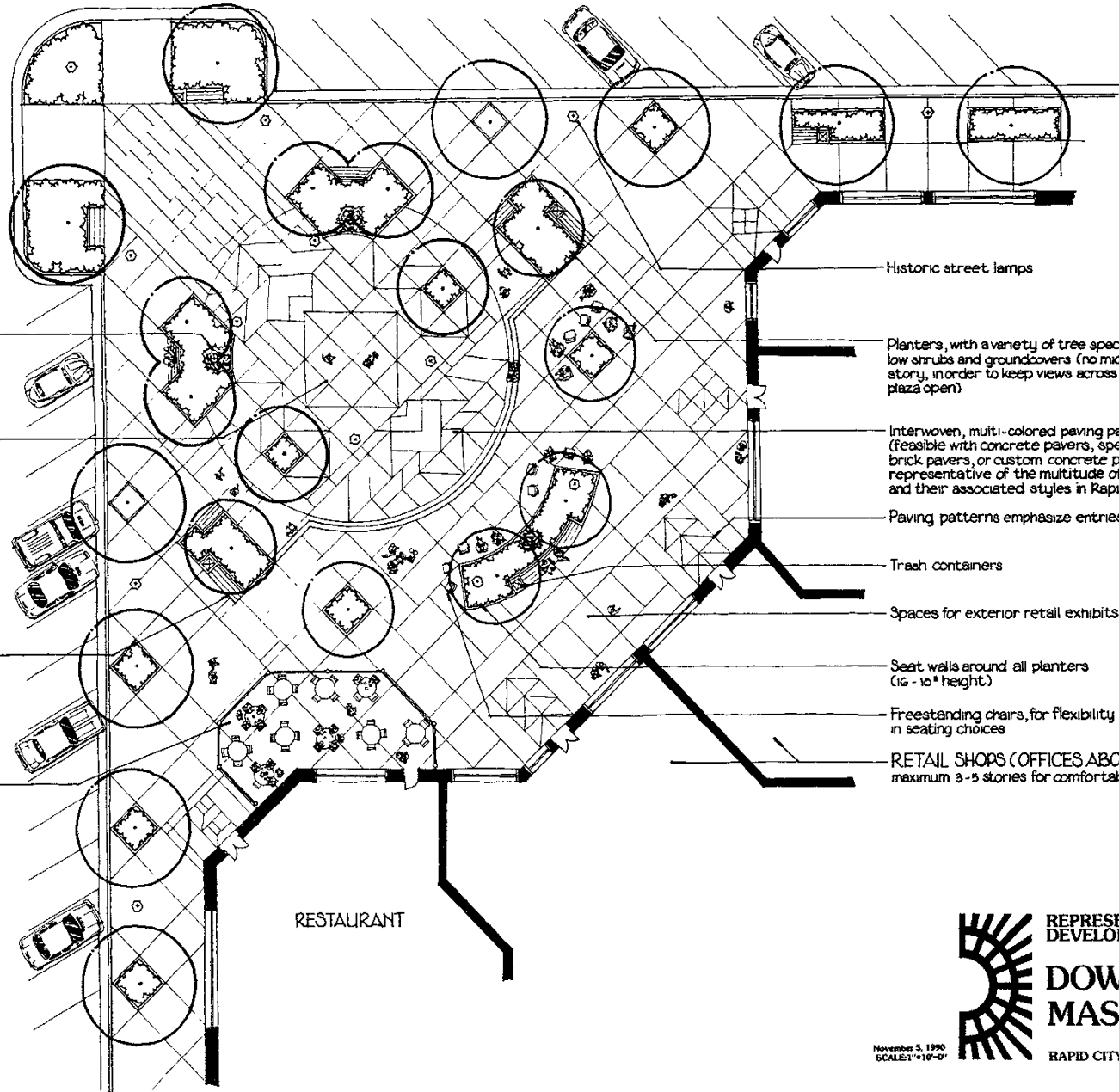
Trash containers

Spaces for exterior retail exhibits

Seat walls around all planters (16 - 10" height)

Freestanding chairs, for flexibility in seating choices

RETAIL SHOPS (OFFICES ABOVE, maximum 3-5 stories for comfortable scale)



REPRESENTATIVE PLAZA DEVELOPMENT

DOWNTOWN MASTER PLAN

RAPID CITY, SOUTH DAKOTA

November 5, 1990
SCALE: 1" = 10'-0"

the value of land in this corridor while encouraging linkages between other high-traffic areas such as the Civic Center and the Main Street/St. Joseph Street retail sectors, thus benefitting a good deal of the Downtown area.

Recommendation:

As opportunities arise, encourage the development of land uses along Sixth Street which cater to and depend on pedestrian traffic- retail, lodging, restaurant, entertainment, and urban plazas. Improve the level of comfort of Sixth Street by installing the suggested streetscape elements.

g. Omaha Street Pedestrian Crossing

Pedestrian crossings of Omaha Street are necessary to facilitate movement between Downtown and several major destinations (Civic Center, proposed museum complex, High School, etc.) north of Omaha Street. In order to encourage this interaction, pedestrian crossings at Fifth Street and Sixth Street should be improved, through favorable timing of signal lights, installation of improved pavement in the crossings, and addition of plant materials and street furniture where possible.

Previous studies prepared for Downtown have recommended that a pedestrian overpass be built over Omaha Street at Seventh Street. While this connection may be physically possible, such overpasses are seldom used by pedestrians when other alternative at-grade crossings are available.

Recommendation:

Improve pedestrian crossings of Omaha Street at Fifth and Sixth Streets. Investment in an overpass should be deferred until such time as is warranted by an overwhelming demand. Such a scenario might occur with development of a large mixed-use project south of Omaha Street which might benefit from a direct connection to Memorial Park.

h. Pavements

The addition of color and texture to pavements will generally create a more interesting ground plane, especially for pedestrians negotiating Downtown streets at a leisurely pace. This can be done in many ways, with the most durable and cost-effective materials being colored concrete, modular concrete pavers, or brick pavers. Other materials which could be considered for small areas include granite or sandstone, provided that snow removal and other maintenance concerns are met. Due to local climatic conditions, any material selected should be relatively high strength, and treatments such as exposed aggregate or stamped concrete should be discouraged, since these surfaces are more subject to cracking and spalling.

As mentioned in "b. Design Considerations", the use of these pavement materials should be somewhat regular in form with noticeable variety in pattern to respond to existing

conditions. This will aid in lessening the perception that the streetscape was one large project.

To add further interest to these pavements, distinctive patterns can be used at the entries to buildings, offering opportunities to add greater detail and personalized expression in the streetscape. Such patterns should reflect the goods and services offered therein, where possible, and must be done in a style and manner that is enduring.

For pedestrian crosswalks, the use of colored concrete is recommended. This can be lightly scored to add interest and would be bracketed by reflective markings to increase safety.

Recommendation:

Increase visual interest by using color and texture in pavements.

i. Plant Materials and Irrigation

The introduction of plant materials into an urban setting creates numerous benefits, both visual and environmental. From a visual standpoint, plants add a layer of pattern, color, and texture to an otherwise "hard" setting. Environmentally, plants aid in comfort control (providing wind screening and shade), dust mitigation, and absorption of some urban pollutants. These benefits will significantly increase the perception that Downtown is a comfortable environment in which to spend more time.

In order to increase the chances of survival for plant materials introduced into an urban environment, and gain the full benefits of their presence, several steps must be taken. First, the area of planting for each specimen or grouping must be as large as is feasible. This increases the rate of exchange of water and oxygen, the two most crucial elements for plant survival. Second, a variety of trees, shrubs, and groundcovers should be used, in order to lessen the potential that insects or diseases could cause extensive damage to the Downtown landscape at any one time. Third, appropriate soil preparation and allowance for sub-drainage is critical. Fourth, a carefully designed irrigation system which delivers water efficiently and at the right time must be installed. And finally, a conscientious year-round maintenance program must be followed.

The use of tree grates is recommended where trees are planted in sidewalk areas less than eighteen feet in width, in order to offer protection to street trees. The grate should be as large as possible (4' x 4' minimum, 5' x 5' preferred) and substantial subsurface work, appropriate for existing soils, should be undertaken to maximize opportunities for root development. This may include use of soil amendments or installation of subsurface drainage, aeration or irrigation systems. The use of pavements in lieu of grates is not recommended, since this creates the most difficult situation for air and water exchange.

As is stated in regard to other streetscape items, plant materials should be planted with some sense of order, but with variety in patterns and placement, to encourage the image of incremental development (in contrast to one large project).

In regard to street trees, use of a variety of sizes and species is especially encouraged. The wide streets call for the use of

large canopy trees in many locations, but the use of smaller ornamental trees is also helpful in adding seasonal interest. These smaller trees might be used most appropriately to screen adjoining parking lots or blank walls of buildings and under overhead lines which cannot be removed.

Recommendation:

Install plant materials (trees, shrubs, groundcovers, annuals, and perennials) in the Downtown streetscape as an aid in increasing visual and environmental quality.

j. Street Lighting

Two types of street lighting are appropriate for Downtown. The first type is a high overhead light which provides a modest level of illumination needed to maintain a safe vehicular and pedestrian environment. This fixture is proposed to be a simple, contemporary fixture which would blend into the background of the urban setting. High lighting efficiency, ease of maintenance, moderate cost of installation, and availability of pole colors to match the remainder of the furniture/signage package should be prime considerations.

The second type of lighting is a pedestrian-scale light, which will serve a number of purposes. It will provide accent lighting, it will add an important layer of vertical elements to diminish the scale of the streets, and it will provide a means of adding other streetscape items, such as banners and/or seasonal flower plantings hung from the lights or seasonal lighting plugged into these lights.

This fixture is proposed to be of historic character with some contemporary features. These lights should be analogous to those already in place, but somewhat simpler in detailing to start a transition away from the existing Historic District. Design considerations should include effective screening of the light source, efficient light distribution, ease of maintenance, moderate cost of installation, and availability of pole colors to match the remainder of the furniture/signage package.

The light source should be matched among all fixtures, with either metal halide or high pressure sodium recommended.

Recommendation:

Incorporate the use of a two-level lighting system, in order to provide a safer, more interesting streetscape.

k. Signage

The discrete use of signage is a key element in providing guidance to Downtown patrons. A clear system of directional and informational signs can guide people to their desired destination and inform them of the rules and regulations appropriate to the areas they are visiting.

At present, there are very few directional signs in Downtown, and there is no consistent system of displaying the few signs that do exist. Due to the high number of tourists and regional visitors who use the Downtown area, a system of signs leading to the primary public destinations is seen as very useful

in orienting these infrequent guests. The location of these proposed signs and the targeted destinations are illustrated on the map on page 35.

At the same time that these signs are being installed, the City should work with the South Dakota Department of Transportation to clarify signage on I-90 regarding Downtown as a destination. At present, it is difficult to make informed choices on the best route to Downtown. The addition of the words "Downtown" or "Central Business District" to existing signs on both the Highway 44 exit on the west and the Highway 16 exit on the east may aid many drivers in making decisions as to the best means to reach Downtown.

The same format used for directional signs is also appropriate for subtle delineation of entries into Downtown. Locations are shown on the same sheet as the directional signage.

In order to reduce the existing clutter of informational signs in Downtown, a second sign type is proposed. This would consolidate signs now mounted on light posts, electrical poles, buildings, and basic T-posts (parking regulations, snow zones, school crossings, etc.) and place them on an upgraded single post, painted to match the remainder of the street furniture package. No more than three signs should be placed on each new post. Finishing touches such as a cap on the post and a mounting collar would substantially upgrade the signage package. The number and location of these consolidated sign posts would be determined on a street-by-street basis.

As part of the consolidation of state routes traversing Downtown, many of the route number signs will be removed from Downtown. Those that remain should be placed either on traffic signal masts or on the informational signage posts described above.

A third sign type, historic plaques, would also be very useful in providing an historic context to visitors. With a current inventory of at least 40 buildings in Downtown designated as significant historic structures, ample opportunity exists to select 6-12 representative buildings and place plaques on them at street level. This collection of plaques should describe the most salient historic points regarding development of this historic regional center.

Recommendation:

Consolidate and enhance the existing selection of signage with a consistent package of directional, entry and informational signs, as well as a package of pertinent historic plaques.

1. Street Furniture

Several items commonly referred to as "street furniture" should be installed as part of a streetscape improvement package in order to increase the level of comfort and convenience for Downtown patrons. These basic items include benches, trash containers, and bicycle racks. Other items such as drinking fountains, decorative fountains, flagpoles, kiosks or other

amenities may be installed at a future date, but these are not considered crucial to the initial success of the streetscape improvements program.

Very few outdoor resting spots currently exist in Downtown. Without these resting spots, the opportunity to recuperate from shopping, enjoy lunch, or socialize with friends is diminished. Therefore, installation of comfortable benches should be strongly considered, at a rate of 1-2 per block face where pedestrian traffic exists or is desired. Benches should have a back and arms for comfort and a strong metal frame for strength. Strong consideration should be given to use of a durable wood for the seat and back in order to provide for maximum year-round comfort.

Trash containers must be available to ease the burden of keeping Downtown clean. These should be strategically placed at heavily trafficked intersections, near benches, and near stores which may generate higher amounts of trash (e.g., carry-out restaurants). The containers should be of a material, style and color compatible with other streetscape elements and should have a liner removable from the side, as well as a lid.

In order to encourage alternative means of transportation, full provisions for bicycles should be made. This includes installation of bicycle racks at areas of heavy usage, such as offices, public buildings, selected retail shops (particularly bicycle shops), and other areas of concentration. These should be compatible in style and color with other streetscape elements and should be placed out of pedestrian lines of travel. The selected design should incorporate strong security features, allowing both wheels to be locked.

Recommendation:

Install street furniture in strategic locations (benches, trash containers, and bicycle racks) to enhance the comfort and function of the Downtown streetscape.

m. Sculpture/Murals/Sound Systems

The package of streetscape items outlined to this point is basic and offers a good, solid basis for an improved urban environment. Several other elements can be added to further improve the image of Downtown, including sculpture, wall murals, and a sound system. Sculpture is particularly appropriate, given the local heritage of Mt. Rushmore, Crazy Horse, and others; pieces should be incorporated in the streetscape nodes, in front of buildings and in other available niches as an accent to the Downtown streetscape. This could initiate a trend which, when tied with the proposed installation of extensive sculptural works in Memorial Park, would begin to establish a strong cultural reputation for Rapid City. This could be seen as a positive aspect by companies seeking to relocate to Rapid City, since such intangible employee benefits can be an influencing factor, while current residents would also benefit.

Another artistic improvement, wall murals, should also be encouraged, particularly on large expanses of blank building walls which might otherwise look somewhat barren. These

murals can depict events, people, or places of public interest while adding color, detail and texture. In addition, as times change, they can be revised at a modest cost. However, the application of murals on historic buildings is not encouraged.

The introduction of a central sound system can also be beneficial, especially when use is limited to special events and for selected seasons such as Christmas. Provision for such a system should be considered for Main Street and St. Joseph Street, from Fifth Street to Mt. Rushmore Road, at each intersection, and for the three proposed plazas.

Recommendation:

Encourage the installation of sculpture, wall murals, and a sound system to improve the cultural and artistic offerings of Downtown.

n. Parking Lot Buffers

Another technique which would improve the visual appeal of Downtown is the use of buffers or screens to block views of parking lots which adjoin sidewalks. Where these lots are to remain, screens composed of shrubs, metal grillwork, or other more substantial materials should be installed to block the view of the first 3-4' in height (primarily up to an average hood height of cars and small trucks). Considerations of available space, cost, appropriate materials, maintenance, sight distances and crime prevention will influence the final selection of materials.

Recommendation:

Encourage installation of visual screens to soften views of parking lots adjoining public sidewalks and encourage extensive landscaping within parking lots.

o. Public Restrooms

Given that many tourists and regional visitors frequent Downtown, it would seem appropriate that basic services provided by a facility such as a public restroom should be made available. This facility could be a component of a new mixed-use project, with a prime location being along Sixth Street, the proposed pedestrian "spine".

Recommendation:

Pursue development of one to two public restrooms in Downtown, in conjunction with mixed-use developments.

p. Utilities

Numerous major and minor utility lines criss-cross the Downtown area, some above-ground and some below. In order to reduce visual clutter, those lines which are above-ground should be buried whenever the opportunity arises. This would include most electrical distribution lines (including those in the alleys) and telephone lines, but may exclude the main electrical transmission line along Omaha, due to prohibitive costs.

Existing underground utility lines, while numerous, are not known to cause any major conflicts at this time. Rehabilitation of some lines will occur with reconstruction of Main Street and St. Joseph Street and storm drainage improvements will be undertaken at several locations where problems now occur.

Recommendation:

Responsible parties should place utility lines underground whenever possible and improve underground lines to meet current standards when existing streets are reconstructed and/or when structures are built or re-built

q. Maintenance

As part of this Master Plan, many physical improvements are proposed to improve the urban environment. While these improvements will be designed with durability in mind, none can be completely maintenance-free. Therefore, it is imperative that funds for maintenance of the streetscape be allocated within the annual budgets of the responsible entities (potentially either the City, a private maintenance district, or a combination of both). Without routine maintenance, the improvements will undoubtedly deteriorate, thus undermining the effort to improve the visual quality of Downtown.

Recommendation:

Prior to installation of improvements, establish an entity responsible for maintenance of streetscape improvements and provide funds suitable for routine upkeep.

r. Management

Installation of an improved physical environment is only one step in the complex effort to draw shoppers and other patrons back Downtown. Along with market studies and marketing efforts to induce appropriate uses to locate Downtown, a comprehensive management program for Downtown is extremely helpful in making the area come alive and keeping it healthy. Management activities could include promotion, organization of year-round festivities, user surveys, security, and financial administration. Should these tasks be neglected, people will not be aware of all that Downtown has to offer and Downtown will not be able to keep up with changing perceptions and demands of their clientele.

Recommendation:

Encourage the development of a strong centralized management entity to administer and promote a variety of programs important to the economic vitality of Downtown.

IMPLEMENTATION

1. **Funding Sources**

In order to fully implement the broad array of programs and improvements recommended for Downtown Rapid City, an extensive and complex funding strategy must be developed. This will call for cooperation between the public and private sectors and will require that a multitude of sources be tapped to carry out the various proposals. Some of the possible sources are listed below:

- a. CITY (Funds, Financial Incentives)
- b. SOUTH DAKOTA DEPT. OF TRANSPORTATION (SDDOT)
- c. SPECIAL AND GENERAL IMPROVEMENT DISTRICTS
- d. PARKING DISTRICTS
- e. TAX-EXEMPT BONDS
- f. BANK COMMUNITY DEVELOPMENT CORPORATIONS (CDC'S)
- g. STATE REDI LOAN
- h. TAX INCREMENT FINANCING
- i. COMMUNITY DEVELOPMENT BLOCK GRANTS (CDBG)
- j. PRIVATE DEVELOPMENT
- k. PRIVATE CONTRIBUTIONS (Buy a Bench)
- l. BENEFACTORS
- m. VOLUNTEERS/GARDEN CLUBS

Each of these sources must be carefully evaluated to understand its applicability to each type of proposed improvement, with any particular project probably drawing from a combination of these sources.

Recommendation:

Utilize a variety of funding sources to spread the burden of financing proposed programs and improvements.

2. Cost Estimates

The following cost estimates offer some perspective on funds needed to construct recommended streetscape improvements. Given that the reconstruction of Main Street is the most imminent project, this is used as a means of demonstrating potential costs (more detailed cost estimates will be made during Design Development and Construction Document phases). Many of the unit costs stated here can also be applied to other near-term projects under consideration.

Preliminary Cost Estimate Main Street Streetscape

<u>Item</u>	<u>Quantity</u>	<u>Unit Cost</u>	<u>Cost</u>
<u>Demolition (1)</u>			
1. Concrete Walks	140,000 S.F.	1.50	\$ 210,000.00
2. Signage	100 ea.	50.00	\$ 5,000.00
Sub-Total			\$ 215,000.00
<u>Pavements (2)</u>			
1. Concrete, Plain	20,000 S.F.	1.75	\$ 35,000.00
2. Concrete, Colored	75,000 S.F.	2.00	\$ 150,000.00
3. Concrete Pavers	40,000 S.F.	5.00	\$ 200,000.00
4. Brick Pavers	15,000 S.F.	11.00	\$ 165,000.00
5. Concrete Edger	2,000 L.F.	6.00	\$ 12,000.00
Sub-Total			\$ 562,000.00
<u>Plant Materials</u>			
1. Shade Tree (2 1/2" caliper)	140 ea.	250.00	\$ 35,000.00
2. Ornamental Tree (2" caliper)	20 ea.	200.00	\$ 4,000.00
3. Shrubs (5 gal.)	750 ea.	15.00	\$ 11,250.00
4. Groundcover (1 gal.)	2550 ea.	5.00	\$ 12,750.00
5. Annuals/Perennials (4")	5000 ea.	2.00	\$ 10,000.00
6. Irrigation (Drip)	L.S.	80,000.00	\$ 80,000.00
Sub-Total			\$ 153,000.00
<u>Street Furniture</u>			
1. Bench	20 ea.	1,000.00	\$ 20,000.00
2. Trash Container	14 ea.	600.00	\$ 8,400.00
3. Bicycle Rack	5 ea.	720.00	\$ 3,600.00
4. Tree Grate	100 ea.	700.00	\$ 70,000.00
Sub-Total			\$ 102,000.00

<u>Signage</u>			
1. Directional Sign	3 ea.	4,000.00	\$ 12,000.00
2. Entry Sign	4 ea.	3,500.00	\$ 14,000.00
3. Informational Sign	60 ea.	250.00	\$ 15,000.00
4. Historical Plaque	4 ea.	1,000.00	\$ 4,000.00
Sub-Total			\$ <u>45,000.00</u>

<u>Electrical</u>			
1. Pedestrian Light	140 ea.	2,500.00	\$ 350,000.00
Sub-Total			\$ <u>350,000.00</u>

TOTAL	=	\$ 1,427,000.00
(3) CONTINGENCY (20%)	=	\$ 285,000.00
		=====
PROJECT TOTAL	=	\$ 1,712,000.00

NOTES

- (1) Demolition and reconstruction of streets, curb and gutter, utilities, and overhead street lights is not included.
- (2) Total pavement cost represents a blend of 1/2 colored concrete, 1/4 concrete pavers, 1/8 brick pavers and 1/8 plain concrete.
- (3) Contingency does not include design fees, financing charges, or other "soft" costs.

3. **Phasing**

In order to establish initial priorities and to maximize benefits from recommended components of the Master Plan, this phasing strategy has been formulated. As is apparent from the list below, many specific timelines will be determined as part of the individual steps noted; therefore, specific recommendations from the Fifth year on are unavoidably vague.

YEARS ONE and TWO

- a. Implement Parking Management Recommendations
- b. Reconstruct Main Street, including streetscapes. Include installation of a signal at Ninth and Main Streets and construction of turn lanes at targeted Main Street intersections
- c. Install Directional and Informational Signage
- d. Install on-street Bicycle Paths on Fifth Street
- e. Commission a Market Study
- f. Initiate a coordinated Marketing Effort
- g. Initiate a coordinated Management Effort
- h. Evaluate City Regulatory controls
- i. Evaluate Mass Transit needs
- j. Initiate discussions with Railroads on crossing gates and improved utilization of right-of-way land
- k. Initiate discussions with Black Hills Power and Light and Landowners on burying of alley utilities
- l. Initiate negotiations with private developers on Mixed-Use/Parking Garage complexes
- m. Encourage implementation of Private Sector Improvements recommended on Supplemental Streetscape Plan.

YEAR THREE

- a. Reconstruct St. Joseph Street
- b. Improve Pedestrian Crossings of Omaha Street at Fifth and Sixth Streets
- c. Install Benches and Trash Containers at strategic locations
- d. Install Sculpture at strategic locations
- e. Encourage installation of parking lot buffers and wall murals
- f. Define priorities for installation of improved streetscapes on north-south streets

YEAR FOUR

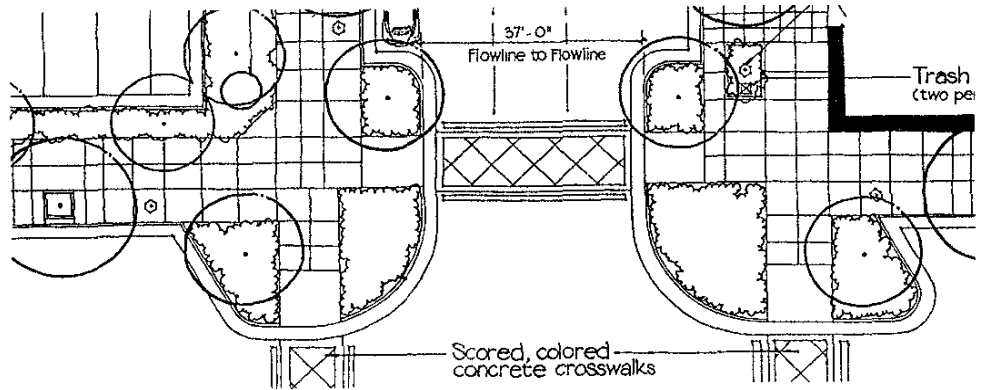
- a. Reconstruct West Boulevard
- b. Install Entry Treatment Signage

YEARS FIVE plus

- a. Implement other components of the Master Plan as appropriate

Tla 108

RAPID CITY



DOWNTOWN MASTER PLAN

STREETSCAPE STANDARDS



January 9
1991

Prepared By:

Bell Galyardt Associates
DHM, Inc.
Leigh, Scott & Cleary, Inc.

Minimum Requirements

All directional and informational signage shall be installed by the City.

Should any historic plaques be installed, they should be produced in the same format, using a durable material such as bronze. Typeface styles and sizes should be coordinated between plaques and with the directional signage. All text shall be subject to review for content and clarity. The City of Rapid City Historic Preservation Commission shall serve as the review entity.

PLANT MATERIALS and IRRIGATION

Discussion

Plant materials can greatly enhance the aesthetic appeal of Downtown while also improving the level of comfort for people and reducing some urban pollutants.

Minimum Requirements

The following shall be minimum sizes for plant material:

Shade Trees	= 2 1/2" caliper
Ornamental Trees	= 2" caliper
Shrubs	= 5 gallon
Groundcovers	= 1 gallon
Annuals/Perennials	= 4" container

In order to provide adequate coverage, all shrubs should be planted at a maximum four feet on-center and groundcovers should be planted at a maximum of eighteen inches on-center.

All plant materials shall be well-mulched, with either a three-inch depth of organic material or a two-inch depth of inorganic material (such as cobble or rock). Inorganic mulches shall not exceed 50% of the visible planted area after plant materials have been installed.

A variety of trees, shrubs, and groundcovers should be used, in order to lessen the potential that insects or diseases could decimate extensive portions of Downtown at any one time. Consideration of plant hardiness, seasonal variation, visual impact, and maintenance concerns should also be taken into account when selecting plant materials. A list of preferred flora is given below. Additional plants may be considered if adequately justified.

Street Trees

Green Ash (Patmore, Summit)
Honeylocust (Shademaster, Skyline)
Littleleaf Linden (Greenspire)
Norway Maple (Emerald Queen)
Red Maple (Red Sunset)
Red Oak

Ornamental Trees

(For use near paved areas: minimal fruit drop)

Canada Red Cherry (Shubert)
Crabapple (Spring Snow)
Amur Maple

Ornamental Trees

(For use in plant beds having a width of eight feet or more, allowing for modest fruit drop)

Crabapple (Red Splendor, Snowdrift, Sparkler, White Cascade,
Zumi Calocarpa)
Cockspur Hawthorn (Thornless)
Newport Plum
Allegheny Serviceberry

Deciduous Shrubs

Alpine Currant
Redtwig Dogwood
Winged Euonymus
Dwarf Forsythia
Emerald Mound Honeysuckle
Lilac (Miss Kim, Dwarf Korean)
Ninebark
Potentilla
Spirea

Evergreen Shrubs

Junipers
Mugho Pine

Groundcovers

Ajuga
Euonymus
Sedum
Vinca

Annuals/Perennials

Numerous Varieties

Appropriate soil preparation and allowance for sub-drainage is critical. Where necessary, overexcavation of planting areas, installation of protective enclosures, addition of soil amendments, installation of perforated drainage pipe or rock wells, or addition of supplemental irrigation systems may be necessary to improve existing conditions.

A carefully designed irrigation system which delivers water efficiently and at the right time must be installed. Such a system must address variable conditions created by sun/shade patterns and fluctuating demand of plants while emphasizing water conservation and low maintenance requirements. Drip, bubbler, or low-volume mist systems with automated controllers are recommended.

A conscientious year-round maintenance program must be followed.

The professional services of a qualified landscape architect should be used in the preparation of all plans.

Additional Guidelines

The area of planting for each specimen or grouping should be as large as is feasible. This increases the rate of exchange of water and oxygen, the two most crucial elements for plant survival. Generally, an area at least five feet square should be provided for an individual tree and an area of at least four feet by twelve feet is recommended for any area with shrub or groundcover plantings around trees, in order to provide room to address the separate irrigation demands of trees and other plant materials.

Free-standing planters which restrict plant materials from rooting into the ground are not encouraged, since such planters require higher maintenance and leave plants more vulnerable to the extremes of winter weather. However, where maintenance for these planters has been assured and planters are only intended for warm weather use, such planters may be allowed.

The use of tree grates is recommended where trees are planted in sidewalk areas less than eighteen feet in width, in order to offer protection to street trees. The grate should be as large as possible (4' x 4' minimum, 5' x 5' preferred), with a maximum 3/8" opening in the selected pattern. Self-weathering steel is the recommended material, due to its durability and low maintenance requirements.

The use of pavements in lieu of grates is not recommended, since this creates the most difficult situation for air and water exchange.

As is stated in regard to other streetscape items, plant materials should be planted with some sense of order, but with modest variety in patterns and placements, to encourage the image of incremental development.